

# Bicycle and Pedestrian Master Plan

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July 2022



City of Hudson

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# Acknowledgements

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# Executive Summary

A master bicycle and pedestrian plan is a document that guides city staff and elected officials in deciding what streets are best for cycling and walking, how to make those streets safer, and what other policies and programs can be made to support improvements to the system. The goals, objectives, and recommended improvements in this plan are based on four key elements: field work, public input, technical analysis, and feedback from city staff and partner organizations such as River Valley Trails, Sustain Hudson, and St Croix Bike and Pedestrian Coalition. The City of Hudson’s Bicycle and Pedestrian Master Plan will serve as a bold first step into the future of walking and bicycling in the city.

## Vision, Goals, and Objectives

The City of Hudson Bicycle and Pedestrian Advisory Committee created a vision statement to establish the framework for the Master Bicycle and Pedestrian Plan: *Walking and bicycling in the City of Hudson is a safe and convenient way for residents and visitors to access all parts of the City, fostering recreation and community connectivity for people of all ages and abilities.*

The Bicycle and Pedestrian Advisory Committee (BPAC) also established four goals to guide the evolution of walking and bicycling in the City:



### Goal 1: Connectivity

*“To develop an interconnected multimodal system that removes infrastructure gaps and allows safer transit to key points within the community.”*



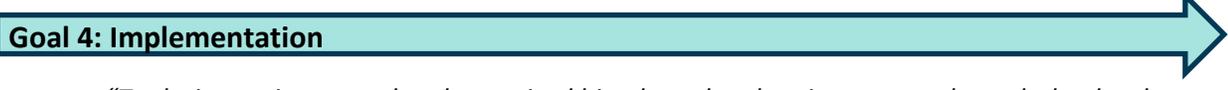
### Goal 2: Safety

*“To provide for the safe, convenient, and accessible movement of people for all modes of transportation.”*



### Goal 3: Health

*“To promote a healthy and active lifestyle amongst all ages and abilities in the community.”*



### Goal 4: Implementation

*“To design an integrated and organized bicycle and pedestrian system through the development of an implementation strategy.”*

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Objectives were also identified for each of these goals and are presented with strategies which include an approach to achievement, a timeline for completion, and key players that should be involved.

## Existing Conditions

City staff reviewed the existing bicycle and pedestrian network as well as existing plans, policies, and programs in the City of Hudson and beyond to understand the current state of infrastructure and culture in the city. Staff identified existing sidewalks, bike lanes, curb ramps, and multi-use trails as opportunities to build upon and expand the network. Other items identified in a review of existing conditions included street signage, crosswalks, and HAWK signals. A review of the physical environment was also performed, which shows that the existing bluff terrain and Interstate-94 provide some barriers that would discourage walking and bicycling.

## Network Analysis and Methods

The existing infrastructure was analyzed to evaluate its suitability for both walking and bicycling. A [Level of Stress Analysis](#) (Furth, 2012) was performed to provide a picture of the quality of Hudson's existing bicycle infrastructure. The results of this analysis showed that while many of the city's off-street multi-use trails offer a low-stress environment for bicycling, arterial and collector roadways and other major barriers create challenges for people traveling by bicycle to school, work, or other daily trips. Additionally, several gaps in the infrastructure cause the system to be ineffective in getting bicyclists and pedestrians to their destinations safely.

Staff and the Bicycle Pedestrian Advisory Committee (BPAC) also [engaged the public for comments on bicycle and pedestrian infrastructure](#) through a series of pop-up booths, surveys, and an online mapping tool. The public identified the following summarized concerns and desires for the network:

- Connectivity between points of interest and residential and commercial areas
- Safety of existing infrastructure
- Maintenance of existing infrastructure, including snow-shoveling and street plowing
- Additional off-road infrastructure (separated bike lanes)
- Safer intersections
- More education and encouragement of walking and bicycling in the community
- More promotion of bicycle tourism
- Improvements to aesthetics, such as path lighting

A [Walkshed Analysis](#) was also performed to determine transport poverty – places with limited mobility options - and the walkability of Hudson's neighborhood resources within a ¼ mile radius (also referred to as the "15-minute walk"). Such resources identified in the assessment include parks and open space, schools and libraries, and places to buy food such as convenience and grocery stores. 5,066 residential properties were used in this analysis, including apartments with several households

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on one tax parcel and excluded multi-use buildings in business zoning districts. An overlay walkshed analysis performed using a geographic information system categorized data into three tiers: Walkable Zones 1, 2, and 3, with Zone 3 indicating those properties that have access to all three neighborhood resource types (food, parks, and education). The following are the results of the analysis:

- **Walkable Zone 1** contains 2,645 residential properties. These properties are within walking distance of one neighborhood resource type.
- **Walkable Zone 2** contains 1,673 residential properties. These properties are within walking distance of two neighborhood resource types.
- **Walkable Zone 3** contains 48 residential properties (52 households). These properties are within walking distance to all three neighborhood resource types.

Further, of the 5,066 residential properties utilized in the analysis, 700 do not fall within any walkable zones. The results of this analysis show that, while many residential properties within the city have access to at least one neighborhood resource, there are very few that have walkable access to two or three. When resources are further away from homes, this will cause residents to form a dependency on motor vehicles to acquire their basic needs (Price, 2018), especially when there are gaps in sidewalk and trail infrastructure.

The information and comments gathered from these existing infrastructure analyses was then integrated into the recommended network.

## Recommendations

The plan includes a comprehensive set of policy, programming, and bikeway recommendations specifically designed to help Hudson become one of the most bicycle-friendly cities in Wisconsin. These recommendations are built on a foundation of public engagement and input and are supported by technical analysis. All recommendations were analyzed from the perspective of “hardware” vs “software” improvements. Hardware is the safe and reliable cycling infrastructure designed to form a complete network (i.e. the “physical” environment). Software components, meanwhile, are about encouraging behavioral change and the mindset of the community around walking and bicycling activities (i.e. the “psychological” environment).

All 22 of the bicycle and pedestrian network recommendations in this plan focus on building an interconnected system of on-street and off-street trails and sidewalks that support safe, comfortable, and convenient travel, regardless of trip purpose. Program recommendations support safe and regular bicycle use among Hudson residents and visitors, as well as respectful and responsible use of public streets and trails by all user types, including people bicycling, walking, and those driving motor

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vehicles. Policy recommendations address the framework for funding, designing and operating the bike network, built on consistent procedures and clear roles for and relationships among city departments and with external agencies and organizations.

## Implementation

To bring this master plan from the planning phase to implementation, funding opportunities, interdepartmental responsibilities, and maintenance recommendations have been laid out. During implementation, it is also important that data is monitored and measured continuously by city staff and the Bicycle and Pedestrian Advisory Committee for plan completion. Lastly, it is staff's recommendation that this master plan be updated every 5 years and continue to correspond with goals listed in the City of Hudson's "Outdoor Recreation" Plan and "Comprehensive" Plan.

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# Introduction & Existing Conditions

The City of Hudson is a growing municipality lying along the western Wisconsin state boundary in St. Croix County, abutting the St. Croix River. The City of Hudson adjoins several neighboring communities, including the Town of Hudson, Town of Troy, and the Village of North Hudson. It lies on the easternmost periphery of the Minneapolis-St Paul Metropolitan Statistical Area (MSA) but also lies within the boundary of the Chippewa Valley-Eau Claire Metropolitan Planning Organization (MPO), the West Central Regional Planning Commission. Because of its location, the City of Hudson is a popular locale for Twin Cities residents to visit due to its small-town charm, scenic natural beauty, quaint downtown with unique shops and restaurants, and ease of accessibility to and from Interstate 94.

## Demographic Profile of the City of Hudson

According to the US Census Bureau Decennial Census, the population in 2020 was 14,755, showing a growth rate of 16% between 2010-2020<sup>1</sup>. By 2030, the Wisconsin Department of Administration estimates that the population will reach 18,810 (Barroilhet, 2013), meaning more than 4,000 new residents are expected to join the community during the implementation of this plan.

Further demographics show that the City of Hudson is 53% female and 47% male based on the 2020 American Community Survey 5-Year Estimates. Likewise, the total population most heavily falls between 20 and 54 years of age (41%)<sup>2</sup>, however, 19.6% of the population is 18 or under, indicating a high population of school-aged youths.

Additionally, it should be noted that 3.9% of the population in 2020 was below 100% of the poverty level, indicating several households that require alternative modes of transportation to work.

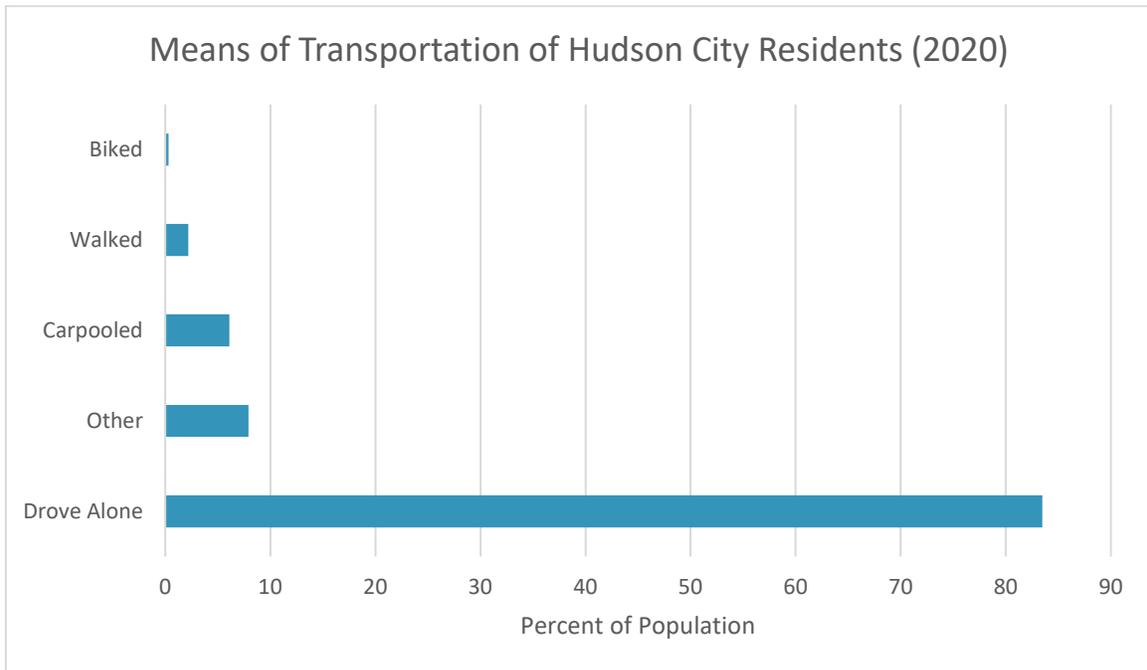
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<sup>1</sup> The population growth rate was calculated by taking the 2020 population (14,755), subtracting the 2010 population (12,719), dividing by the 2010 population, and then multiplying by 100 to get a percent value.

<sup>2</sup> According to the 2016-2020 American Community Survey 5- Year Estimates from the US Census Bureau, residents aged 20-29 years old make up 14.9% of the population, residents aged 30-39 years old make up 13.7% of the population, residents aged 40-49 years old make up 12.2% of the population and residents aged 50-54 make up 7.3% of the population. Residents 24 years and under make up 30.2% of the population. And residents aged 55 and older make up 28.9% of the population.

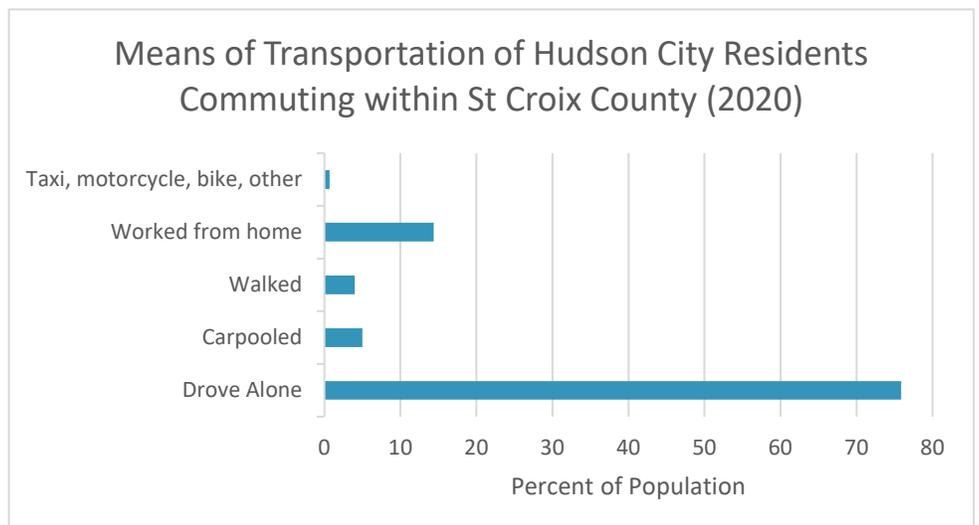
## Transportation Demographics

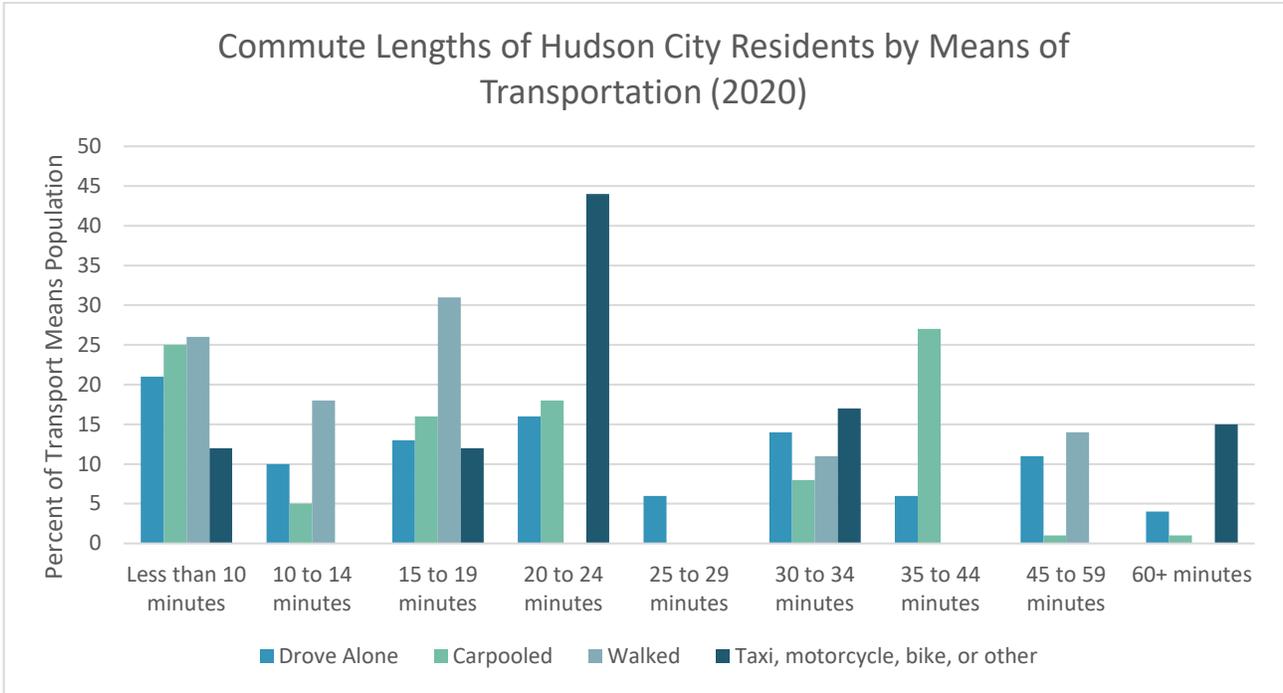
From the Census Bureau's 2016-2020 American Community Survey 5-Year estimates indicates that in 2020, 83.46% of City residents drove alone to work. Another 6.1% carpooled, 2.2% walked, and 0.3% biked. The "other" transportation option consists of working from home (7.5%), as well as the use of motorcycles, buses, and taxis.



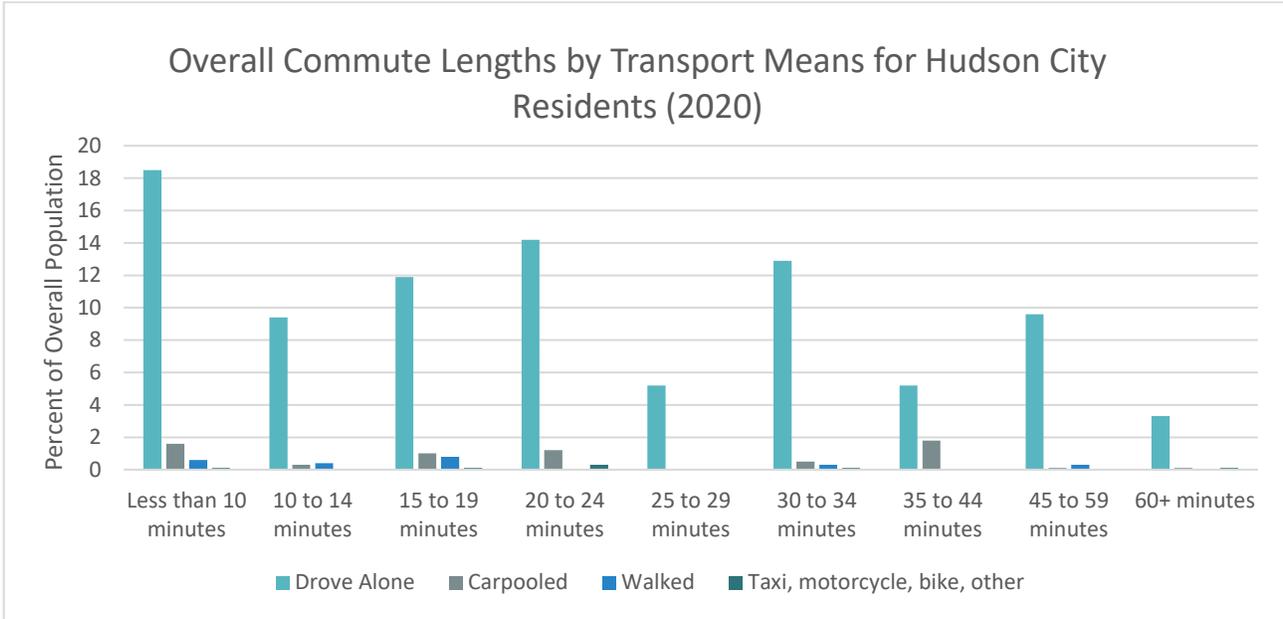
From the same 2020 data, City of Hudson residents worked primarily (54.4%) within Wisconsin and 95.5% of those worked within St Croix County. However, 45.6% of total residents worked outside of Wisconsin.

Of local Hudson City residents commuting to jobs within St Croix County, 75.9% drove alone, 5% carpooled, 4% walked, and 0.7% used a bicycle/motorcycle/taxi or other non-public transport option. The last 14.4% of Hudson City residents within St Croix County worked from home in 2020.





Commute lengths are an important figure to mention, especially regarding those residents who have commutes less than the average of 21.3 minutes in 2020 or for those with more “walkable” workplaces within a 15-minute walk of their residences. Note that the commute lengths chart above does not break down commute lengths of each type of transport method from the overall population, but rather is broken down by transport means. For example, the first bar from the left is interpreted as “21% of City of Hudson residents in 2020 who drove to work alone had a commute of less than 10 minutes”. For comparisons between method of transport and length of commutes from overall city population, please refer to the chart below.



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## Elderly and Disabled Residents

According to the US Census Bureau's 2016-2020 American Community Survey Estimates, 10.8% of total noninstitutionalized residents in the City of Hudson live with a disability. Additionally, the US Census Bureau estimates that the City of Hudson's elderly population (those aged 65 and older) in 2020 was approximately 19.4% of total residents.

As advances in medical technology continue to emerge, people will live longer and be more likely to recover and thrive from conditions that previously would have done much more harm. In this regard, communities such as Hudson have an important role to play in prioritizing the mobility and safety of all ages, including that of older adults. Physical activities such as walking and bicycling provide benefits to the elderly in the form of building physical strength and endurance, increasing balance, providing an improved sense of community and place, and helps to prevent feelings of social isolation.

Additionally, providing ADA-inclusive infrastructure for the City's disabled population, which comprises those with physical impairments, hearing loss, and vision loss, helps everyone on a broader scale (Bellis, 2020). The reality is that anyone can become disabled any day and at any time due to unforeseen circumstances. Proactively building infrastructure as required by the Americans with Disabilities Act will open doors and make life easier for all, allowing everyone to enjoy the City of Hudson and its offerings as intended.

## Past Plans & Existing Infrastructure

The Hudson Bicycle and Pedestrian Advisory Committee has been a part of the City since March 30th, 2015. Since the time of its adoption, however, a formal plan was never put in place for how the City would achieve its goals. Rather, most of the time was spent going over maps with the group to determine which areas posed more challenging than others that could benefit from bicycle and pedestrian improvements such as future bike parking, bike ‘fix-it’ stations, benches and more.

Updates and improvements to the bicycle and pedestrian system have largely been done through Capital Improvements Programming via the Parks and Public Works Department, with new trail additions being made in conjunction with the Community Development Department through new residential and commercial developments. Existing multi-use trails in the City of Hudson include:

- ❖ [Heggen Street Trail](#) (0.68 miles)
- ❖ [Lakefront Park Trail](#) (3.56 miles)
- ❖ [Crestview Drive Trail](#) (1.12 miles)
- ❖ [Coulee Road Trail](#) (1.40 miles)
- ❖ [Hanley Road Trail](#) (2.23 miles)
- ❖ [Carmichael Road Trail](#) (2.89 miles)
- ❖ [Vine Street Trail](#) (1.88 miles)
- ❖ [Grandview Drive Trail](#) (1.61 miles)
- ❖ [Hunter Hill Trail](#) (0.32 miles)
- ❖ [Coon’s Hill Park Trail](#) (0.71 miles)



Image 1. View of Water Tower from Hanley Road Multi-Use Trail

The City of Hudson has done well over the years by ensuring new developments incorporate trail systems and sidewalks while also maintaining connections in the older neighborhoods of the City. With 7.8 square miles of area, the City currently boasts approximately 74 miles (391,196.38 feet) of bicycle and pedestrian infrastructure, including multi-use trails, sidewalks, and bike lanes. Although this number is impressive, the lack of safe connections between trail routes and sidewalks noted by residents, as well as a lack of knowledge that these trails exist, has spurred the need to draft a Master Bicycle and Pedestrian Plan that is refined to the City’s needs. A map of this existing infrastructure can be found in Appendix C-4.

As there are other plans and studies (St Croix County Bike Ped Plan, Hudson Outdoor Recreation Plan, 2021 Comprehensive Plan, etc) that have recently analyzed the City of Hudson’s bicycle and pedestrian infrastructure, an additional goal of this Master Bicycle and Pedestrian Plan will be to incorporate those goals from existing plans to create an interconnected and thorough path toward common goal attainment.

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## Safe Routes to School

Beginning in 2014, the Hudson School District partnered with the West Central Wisconsin Regional Planning Commission, the City of Hudson, and the Village of North Hudson to draft the district's 2018 Safe Routes to School Plan. The plan envisions the following future:

*"The Hudson School District, City of Hudson, and Village of North Hudson have safe and enjoyable environments for walking and biking to and from school. To allow walking and biking as viable options for school children and the broader community, the School District, City, and Village collaborate with other community stakeholders in enhancing and increasing interconnected walking/biking systems to and at the schools. Safe walking, biking, and driving habits; a family-oriented SRTS environment; and development that incorporates SRTS initiatives are common in the City and Village."*

The plan itself encompasses recommendations for each school based on the Six E's Strategy: Engineering, Education, Encouragement, Enforcement, Equity, and Evaluation. Recommendations related to the City of Hudson under each of the six E's were the following:



### Education

- ✓ continue to strengthen the student safety patrol program
- ✓ promote SRTS on website and social media
- ✓ work with the local media in promoting SRTS
- ✓ incorporate SRTS into local neighborhood watch programs
- ✓ work with local driver education programs to stress the importance of driving safely around schools and in School Zones.
- ✓ Hold annual best practice trainings for Student Safety Patrol supervisors and adult crossing guards.



### Encouragement

- ✓ require all adult and student crossing guards to wear safety yellow clothing
- ✓ keep adult crossing guards at current locations and research needs at additional locations
- ✓ work with police department with school-specific or district-wide bike rodeos
- ✓ create an environment where adult crossing guards feel more connected to the school(s) where they work
- ✓ distribute reflectors for backpacks
- ✓ hold meetings twice a year with adult crossing guards, school staff, and police departments at each respective school
- ✓ collaborate with community organizations and private businesses to help promote walking and biking to/from school
- ✓ provide annual training for adult crossing guards to review best practices and policies



## Enforcement

- ✓ investigate legal or nuisance activity that currently deters walking and biking to/from school
- ✓ locate police officers around schools or other pertinent SRTS locations before and after school
- ✓ enforce all parking/pick-up areas in front of school and on school property
- ✓ increase speeding fines in school zones and post the fine cost on School Zone signs
- ✓ do not allow idling cars in school zones



## Engineering

- ✓ install traffic calming measures around schools to significantly reduce vehicle speeds in School Zones and along SRTS corridors
- ✓ collaborate to find solutions for students to be able to walk and bike safely from Hudson Middle School and Prairie Elementary School to the YMCA
- ✓ incorporate walking lanes or sidewalks with appropriate signage along SRTS corridors
- ✓ create a consistent uniform and signage for safety patrol and crossing guards



## Equity

- ✓ continue to incorporate SRTS programs
- ✓ implement SRTS policies that work with and provide for all demographic groups



## Evaluation

- ✓ continue to meet as an SRTS Task Force twice a year
- ✓ update City committees on SRTS issues, opportunities, and progress

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## St Croix County Bike Ped Plan (2017)

On May 2<sup>nd</sup>, 2017, the St Croix County Board of Supervisors approved Resolution 17 to adopt the County's Bicycle and Pedestrian Master Plan. The plan was prepared by the County's Community Development Department and Highway Department, Toole Design Group, and the West Central Wisconsin Regional Planning Commission. The plan suggested several key recommendations and strategies:

- 1) **Enhancing the network along route recommendations with specified improvements. For the City of Hudson, specific project recommendations included:**
  - Varied facility recommendation between I-94 Bridge to River Crest Elementary School Connection (4.2 miles with a cost estimate of \$1.08 million). This project was identified in the county plan as an early action "selected priority corridor", indicating that this pathway is detrimental to filling a transportation gap and would provide a key linkage for local users.
  - Bike lane facility recommendation along Buckeye Street (between 1<sup>st</sup> and 2<sup>nd</sup>) in downtown Hudson (0.1 miles with a cost estimate of \$3,000)
  - Bike lane facility recommendation along 9<sup>th</sup> Street between Orange Street and Fox Street (0.0 miles with a cost estimate of \$1,000)
  - Path facility recommendation along Vine Street between 10<sup>th</sup> Street and Wisconsin Street (0.4 miles with a cost estimate of \$185,000)
- 2) **Outlining the planning, budgeting, and right-of-way acquisition process**
- 3) **Coordination and Communication policy recommendations** such as an intergovernmental annual or biannual summit, quarterly newsletter, training and continuing education, increased public engagement, outreach and education, tourism and economic development, and child encouragement and safety.
- 4) **Design and Implementation policy recommendations:**
  - County Development Requirements Modifications
  - Municipal Development Requirements Modifications
  - Contracts and Contractor Oversight
  - Use of Design Guidelines
  - WisDOT Resolution of Support
  - Municipal Complete Streets Policies (with a model policy provided)
  - Sustainable Maintenance Strategy and Program
- 5) **Municipal funding strategy recommendation where municipalities will be responsible for securing funding.** However, if requested, the County may provide each municipality with a specific funding goal based on factors such as mileage of recommended improvements within their jurisdiction, the community's property valuation, current/forecasted population, etc.

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## People Walk with Purpose: A Land Use Summary

When residents traverse the city, regularly they do not do it aimlessly or without purpose. Oftentimes, they are travelling to a destination point: school, grocery store, convenience store, library, post office, etc.

Currently in 2022, the City of Hudson’s land area consists of 3,536.01 total acres. Land uses are spread out across the city within the following land use categories:

<b>LAND USE</b>	<b>ACRES</b>	<b>% of City</b>
Public	173.07	4.9%
Park	232.86	6.6%
Commercial	596.51	16.9%
Rural, Single & Two Family Residential	1,177.83	33.3%
Multiple Family Residential (3+ dwellings)	489.38	13.8%
Institutional	278.20	7.9%
Industrial	558.27	15.8%
Conservancy	29.89	0.8%

With the top land use categories being low family residential, commercial, and industrial land uses, the potential for bicycle and pedestrian traffic generation is heavily dependent upon what is nearest to these uses. As the city’s land use is directly linked with its Euclidean Zoning style, this draws a heavy geographic separation between the major differing land use types and encourages the use of private automobiles (Sonta & Jain, 2019). Residents are thereby less inclined to walk or bike unless a certain resource or business type is nearby (Sonta & Jain, 2019). Unfortunately, this is a commonly occurring problem amongst suburban communities across the United States as many communities similar to Hudson adopted Euclidean-style zoning codes from 1926 to the present day. Thankfully it is possible to retrofit existing districts with improvements and land uses to support higher levels of walking and bicycling.

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## Policy Context

Existing City of Hudson municipal code outlines requirements for bicycle and pedestrian use in dated terms. Presently, [Chapter 235 Section 5](#) discusses bicycle use within the city as well as proper maintenance, operation, and care. This section of code, as it was last amended in 1991, has *not* been updated to reflect modern technologies such as E-bikes.

Additionally, under [Chapter 212 Article 2](#), City of Hudson municipal code delves into the definition and requirements of bicycle and pedestrian-related events such as parades, marathons, and races. In the event the City of Hudson were to host such events, a permit application would be required and would be reviewed by the Public Safety Committee and police department with final approval or denial obtained from the Common Council.

[Chapter 255 Section 29](#) discusses Public Convenience Structures such as benches, trash bins, bicycle racks. This section explains that such structures shall not be located in any public right-of-way unless approved by certificate of compliance through the Community Development Department. This includes on public sidewalks such as, for example, in the downtown central business district.

Under [Chapter 235 Section 1](#), a chart was adopted and updated in 2000 to reflect existing statutory requirements of traffic laws, including those related to bicycles and pedestrians. Such laws included in this section are:

- 6) Overtaking and passing bicycles
- 7) Responsibility of parent or guardian for violation of bicycle and play vehicle regulations
- 8) Special rules applicable to bicycles
- 9) Riding bicycle on roadway
- 10) Riding bicycle on bicycle-way
- 11) Riding bicycle on sidewalks
- 12) Lamps and other equipment on bicycles
- 13) Blind pedestrian on highway
- 14) Pedestrians to walk on left side of highway; pedestrian and bicycles on sidewalks
- 15) Pedestrian control signals
- 16) Crossing at places other than crosswalks
- 17) Crossing at uncontrolled intersections or crosswalks
- 18) Crossing controlled intersections or crosswalks

[Chapter 165 Section 1](#) discusses curfew hours between 11 p.m. and 5 a.m. for minors aged 16 and under without parent or legal guardians present. This requirement denotes all minors “*on foot, bicycle, or any type of vehicle...*”

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Chapter 212 Section 9 ordains that, for public safety reasons, sidewalks must be kept clear of snow and ice during the winter months, within 24 hours after a continuous period of snowfall. This code section also defines where excess snow shall be deposited as well as ramifications if a property owner or occupant does not comply with this subsection.

Chapter 212 Section 11, although not explicitly about bicyclists and pedestrians, it does define requirements for the use of skateboards, roller skates, roller skis, rollerblades, and other play vehicles. This regulation is provided in municipal code partly as a means of protecting the flow and safety of pedestrians on sidewalks.

Lastly, the Downtown Overlay District outlined in Chapter 255 Section 17.10 describes architectural design elements that would entice more pedestrian traffic in and around the central business district area of the city.

In summary, municipal code touches on the topic of bicycle and pedestrian modes of transport throughout the city. However, the code does not define requirements within new development sites to necessitate developers to install trail extensions or bicycle parking. Similarly, there are no sections of code for design requirements pertaining to sidewalks or bicycle trails, leading to erratic design and disconnected trails. The inclusion of technological advancements in bicycle/pedestrian modes of transport are also a necessity in any future code amendments.

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## Opportunities and Constraints

More development is occurring rapidly with new land annexations into the City of Hudson. The Southpoint, Summit Ridge and Park Place developments to the south of town are pushing the City's infrastructure further south. Likewise, the Carmichael Road corridor just north of I-94 is also experiencing annexation pressures on the northeast part of town, including the recent annexation of the Bella Rose development at the corner of Carmichael and Vine. These external pressures are causing the City to slow down and reevaluate the connectivity between neighborhoods and community resources. More recently, the city has been strongly advising developers to include pedestrian and bicycle facilities into their development plans to create better connections throughout the city by filling in the gaps. However, despite the added connections in newer neighborhoods, there are still some gaps.

### Constraints:

- 1) City's growth on the east and south boundaries pushes resources and infrastructure further out from the center.
- 2) City's bluff line that separates west from east.
- 3) I-94 that runs through the center of the City, splitting it between north and south.
- 4) Motor vehicle dependency of residents and visitors currently takes a lot of on-street parking both in downtown and in residential neighborhoods.
- 5) City code requires an abundance of parking to be provided on all new developments, taking away valuable land resources and further inducing urban sprawl.

### Opportunities:

- 1) Waterfront Vision Study drafted in February 2018 plans for the provision of streetscape (trees, pot planters, etc) and intersection improvements (Walnut and Commercial), a transient dock, floating boardwalk, trailhead installation, community center and outdoor garden development, and other notable improvements of existing amenities in Lakefront Park. With these improvements, Lakefront Park would become a bigger tourism driver and economy booster and would thereby increasingly promote greater use of downtown bicycle and pedestrian infrastructure.
- 2) The Lift Loop Trail has been officially opened to the public and has drawn tourism to the St Joseph/Stillwater/Houlton region to the north of the City of Hudson. Connections to the Village of North Hudson would assist with the city linking closer to this major multi-state recreational trail route.
- 3) As a tourism asset, the City of Hudson is one of several communities that are part of the Yellow Stone Trail (the first Transcontinental Automobile Highway route created by a group of small-town businessmen in 1912 that spanned across the northern United States from the east coast to the west).
- 4) Many local roads in the City of Hudson contain road widths varying between 60-66 feet in width, providing ample room for the City to construct or "test-run" bicycle/pedestrian infrastructure by means of Tactical Urbanism methods.

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# Vision, Goals and Objectives

## Guiding Principles

The purpose of the Bicycle and Pedestrian Master Plan is to serve as a guide to making the City of Hudson more accessible and accommodating for bicyclists and pedestrians of all ages and abilities, from young children to senior citizens, and from beginning to experienced riders. Hudson strives to be a beautiful, active, and functional community with both a fantastic trail system and wonderful parks. The Bicycle and Pedestrian Master Plan can serve as a guide to improving these existing amenities, while also offering well-researched recommendations for new pathways in the future. A community that encourages exercising – whether it be running, walking or bicycling - is a community that not only promotes physical wellbeing, but also recognizes the social, economic, and environmental benefits. Providing connections to destinations throughout the city and to neighboring communities can make the City of Hudson a better place to live, work and play for generations to come.

## Vision and Goals for the Plan

The Vision and goals for the City of Hudson’s Bicycle and Pedestrian Master Plan were developed through a combination of local government research and public input. Through public input, the Bicycle and Pedestrian Advisory Committee was able to conclude which issues residents were most concerned with. Simultaneously, local government research helped to develop some overlapping intergovernmental goals with Hudson’s neighboring communities. Research also included time spent reviewing regional bicycle and pedestrian plans from the Wisconsin Department of Transportation as well as St Croix County.

### Vision Statement

*“Walking and bicycling in the City of Hudson is a safe and convenient way for residents and visitors to access all parts of the city, fostering recreation and community connectivity for people of all ages and abilities.”*

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## Goal 1. Connectivity

To develop an interconnected multimodal system that removes infrastructure gaps and allows safer transit to key points within the community.

- ❖ Objective 1: Develop a safe trail connection to North Hudson.
- ❖ Objective 2: Install wayfinding features to promote the use of major pedestrian thoroughfares.

## Goal 2. Safety

To provide for the safe, convenient, and accessible movement of people for all modes of transportation.

- ❖ Objective 1: Proactively repair aging sidewalks, trails, and painted pedestrian lanes.
- ❖ Objective 2: Add more 'Pedestrian Crossing' signage and other safety infrastructure on major roadways.
- ❖ Objective 3: Promote the use of high-visibility clothing, lights, and helmets through public outreach initiatives.

## Goal 3. Health

To promote a healthy and active lifestyle amongst all ages and abilities in the community.

- ❖ Objective 1: Add more bicycle parking racks in downtown Hudson.
- ❖ Objective 2: Create maps and other information products that entice residents to be adventurous by showing routes of varying lengths and themes such as a nature tour, history tour, and other points of interest.

## Goal 4. Implementation

To design an integrated and organized bicycle and pedestrian system through the development of an implementation strategy.

- ❖ Objective 1: Create a budget and maintenance plan of pathways for year-round use.
- ❖ Objective 2: Perform a study to determine and promote 1-2 "best" routes for pedestrians to travel across major geographic and/or roadway barriers:
  - **Hudson South of I-94 and north of I-94 (north to south)**
  - **Hudson "up the hill" and downtown (east to west)**

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# Needs Analyses and Methods

Quantitatively scoring the existing infrastructure for bicycle and pedestrian trails, bike lanes, and sidewalks involved various types of analysis and methods. For pedestrian scoring, a walkshed analysis was performed. For bicycle scoring, a Level of Traffic Stress Analysis was performed. And lastly, as a means of grading existing infrastructure for both user types, a crash analysis study was performed. Below are the summaries of all analysis methods and their respective results for the City of Hudson's infrastructure.

## Pedestrian Walkshed Analysis

To determine how walkable community resources within the City of Hudson are, a walkshed analysis was performed. This analysis involved calculating a distance of ¼ mile radii around each resource, indicating a 5-minute walking distance, which studies from the Center for New Urbanism show is the distance most pedestrians are willing to walk (Andres Duany, 2021). Community resources included schools and libraries, parks and open spaces, and grocery or convenience stores. The significance of these resources is that they all contribute to a community's basic well-being as residents need fresh air and exercise, food, and education.

The analysis breaks down the results into 3 Walkable Zone types. Zone 1 indicates properties that have access to only one type of community resource. Zone 2 indicates properties that have access to two types of community resources. And Zone 3 indicates properties that have access to all three categories of community resources (grocery, parks, and education).

5,066 residential properties were analyzed, including several apartments and condominiums with more than one household per tax parcel. The analysis did not include those residences that fall within multi-use buildings such as those in Business Districts.

### RESULTS

Of the 5,066 residences, 2,645 residential properties fell within Walkable Zone 1. This indicates that a little over half (52%) of city residents have walkable access to at least one community resource. Of those 2,645 residential properties, 1,673 are within Walkable Zone 2 and have access to at least two community resources. And of those 1,673 residential properties, only 48 (~1%) fall within Walkable Zone 3. These 48 residential properties include 52 households total. This Zone primarily has access to EP Rock Elementary School for education, RJ's Meats, a Holiday convenience store, Fresh & Natural Foods, and Dollar Tree for food options, and Birkmose Park, Coon's Hill, and Cudd's Golf View Open Space for parkland.

Most significantly, the analysis revealed 700 properties (13.8%) do not fall within any walkable zones, meaning these residences do not provide much opportunity or reason to walk anywhere.

A map of the Walkability Zones Analysis is included in Appendix C-1.

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# Bicycle Level of Traffic Stress Analysis

Bicyclists are a demographic that can be comprised into 4 categories:

- 1) **The Strong and Fearless** are those cyclists who will ride regardless of roadway conditions and consider riding a bicycle as a strong part of their identity.
- 2) **The Enthused and Confident** are those cyclists who are comfortable riding on a road with automobiles but prefer to do so operating on their own facilities such as a dedicated bike lane. These individuals appreciate efforts made to improve the bikeway infrastructure.
- 3) **The Interested but Concerned** are those who are curious about bicycling, and like to ride, but are afraid to do so and therefore do not ride regularly.
- 4) **The No Way No How** are those who refuse to ride a bicycle typically for reasons related to topography, inability, or a lack of personal interest.

These four categories, created by the City of Portland's Bureau of Transportation Bicycle Coordinator Roger Geller, help to identify which level of interest bicyclists fall into. These broad categorizations have helped cities across the United States demonstrate why further investments into local bicycle facilities would encourage more bicyclists to use the infrastructure. It's not about broadening the system as much as it is making things more comfortable for the everyday user.

Level of Traffic Stress is a rating system in which a value is given to a road segment or crossing indicating the traffic stress it imposes on bicyclists. The ratings include four levels:

- 1) **LTS 1** is a rating given to roads that exemplify the safest and simplest crossings and are generally suitable for children. They are indicated by a strong separation from all except low speed, low volume traffic zones.
- 2) **LTS 2** is a rating given to roads that are deemed tolerable by most bicycle-riding adults, particularly those individuals that are classified as "**interested but concerned**". LTS 2 zones are indicated by cyclists having their own place to ride that keeps them from having to interact with traffic except at formal crossings and low speed/low volume traffic situations. These zones may also include physical separation barriers from higher speed and multilane traffic. Crossings under this category are easy for an adult to negotiate, but may not be suitable for children.
- 3) **LTS 3** is a rating given to situations where bicyclists are interacting with moderate speed or multilane traffic. This is generally considered to be a level of traffic stress acceptable to individuals who are classified as "**enthused and confident**" bicycle riders.
- 4) **LTS 4** is a rating given to situations where bicyclists are interacting with higher speed traffic or are within close proximity to highspeed traffic. This is a level of stress that's generally considered acceptable only to those who are classified as "**strong and fearless**".

## RESULTS

The Level of Traffic Safety (LOTS) analysis for the City of Hudson found that most road corridors have a level 1 rating, meaning most roads in the City of Hudson are deemed safe and include simple crossings and occur in slow speed, low volume traffic zones. A few corridors have a level 2 rating, indicating that these roads may be tolerated by adults, but may not be suitable for children. Concerning roadways that could use improvements for bicycle and pedestrian users include the

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following, all of which fall under Level 3 and 4 ratings:

- Carmichael Road
- Coulee Road between Carmichael and 11<sup>th</sup> Street
- Crest View Drive between O’Keefe and Industrial (a key node in which several traffic incidences have been reported)
- Ward Avenue between Hillcrest Drive and 17<sup>th</sup> Street
- 17<sup>th</sup>, 18<sup>th</sup>, and 19<sup>th</sup> Streets between Ward Avenue and Coulee Road
- Badger Drive
- Gateway Boulevard from Badger Drive to Gateway Court
- County Road F
- Industrial Street
- O’Keefe Road
- Hanley Road between Heggen Street and Carmichael Road
- Stageline Road
- Webster Street
- Pearson Drive

The above are roadways and gaps that only the confident or fearless bicyclist would use to traverse the City of Hudson. Many of these corridors create concerning situations for residents attempting to use the city’s existing infrastructure for bicycling. Due to this, the above list will be compared with other data to comprise a list of potential improvements so that all bicyclists feel safer on their journeys.

A map of the Bicycle Level of Traffic Stress Analysis is provided in Appendix C-2.

## **Collision and Safety Analysis**

Further analyzing the city’s existing bicycle and pedestrian infrastructure, it is important to review problem areas within the city that are likelier to induce crashes, whether due to engineering design or by human behavior. City staff requested crash data from the University of Wisconsin Madison’s Traffic Operations and Safety (TOPS) Laboratory. The TOPS laboratory provides crash data to the Wisconsin Department of Transportation Bureau of Traffic Operations and houses a crash database that is provided by the Wisconsin DOT’s Division of Motor Vehicles (DMV). This crash database contains information on all police reported crashes in Wisconsin from 1994 to the current year. Detailed information such as the location of crashes, vehicles involved, weather conditions, and other general crash attributes are available. However, personal identifying information is removed for privacy purposes. Lastly, it is important to note that not all reportable crashes are reported. For a crash to be in the database, an MV4000 crash report must have been completed by a police officer.

For this master plan, crash data was requested for the timeframe of January 2010 to January 2021 within the City of Hudson. This data was then mapped in ArcGIS as point data. From there, the data

was used to generate a “heat map”, showing which locations in the city have higher occurrences of crashes than others. Heat regions were calculated by measuring the closeness between each point. The final heat map of these crash occurrences can be found in Appendix C-3. In summary, the heat map shows crashes generally occurring most frequently along 2<sup>nd</sup> Street/Highway 35 in downtown, as well as along Crest View Drive and Carmichael Road. Below is a table ranking all crash sites

<b>Crash Occurrence Sites (ranked by frequency)</b>		
<b>Rank</b>	<b>Intersection of Crash Occurrence</b>	<b># of Occurrences</b>
1	Vine and 2 <sup>nd</sup> Street	3
1	Locust and 2 <sup>nd</sup> Street	3
2	Walnut and 2 <sup>nd</sup> Street	2
2	Crest View Drive and Industrial Street	2
2	Crest View Drive and Heggen Street	2
2	Carmichael Road and Center Drive	2
2	Carmichael Road and Burl Oak Curve	2
2	St Croix and 2 <sup>nd</sup> Street	2
2	11 <sup>th</sup> Street and Laurel Avenue	2
3	Walnut and 3 <sup>rd</sup> Street	1
3	St Croix and 7 <sup>th</sup> Street	1
3	North and 2 <sup>nd</sup> Street	1
3	Coulee and 2 <sup>nd</sup> Street	1
3	Front Street (under I-94)	1
3	11 <sup>th</sup> Street S (between Colonial & Green)	1
3	13 <sup>th</sup> Street and Ward Avenue	1
3	17 <sup>th</sup> Street (between Coulee Road & Ward Avenue)	1
3	17 <sup>th</sup> and Summer Street	1
3	Locust and 4 <sup>th</sup> Street	1
3	Vine and Wisconsin Street	1
3	Carmichael Road and Oakridge Circle	1
3	Galway Court and Burl Oak Curve	1
3	Grandview and Spruce Drive	1
3	13 <sup>th</sup> and Wisconsin Street	1
3	Crest View Drive and O’Keefe Road	1
3	Webster Street and O’Keefe Road	1
3	Crest View Drive and Gateway Boulevard	1
3	Hanley Road and Pearson Drive	1
3	Heggen Street and Mont Croix Drive	1
3	Hanley and O’Keefe Road	1
3	Mayer and O’Keefe Road	1
3	Industrial Street and Livingstone Road	1
3	East Canyon Drive and Loughney Bay	1
3	Bridgewater Trail	1

Further details pertaining to these crash occurrences, such as weather conditions at the time of the crash, time of year, and speed limits were derived to better understand circumstances that may have contributed to these crashes.

## CRASH ANALYSIS INSIGHTS

Between January 2010 and April 2022, the City of Hudson averaged

**4.8**

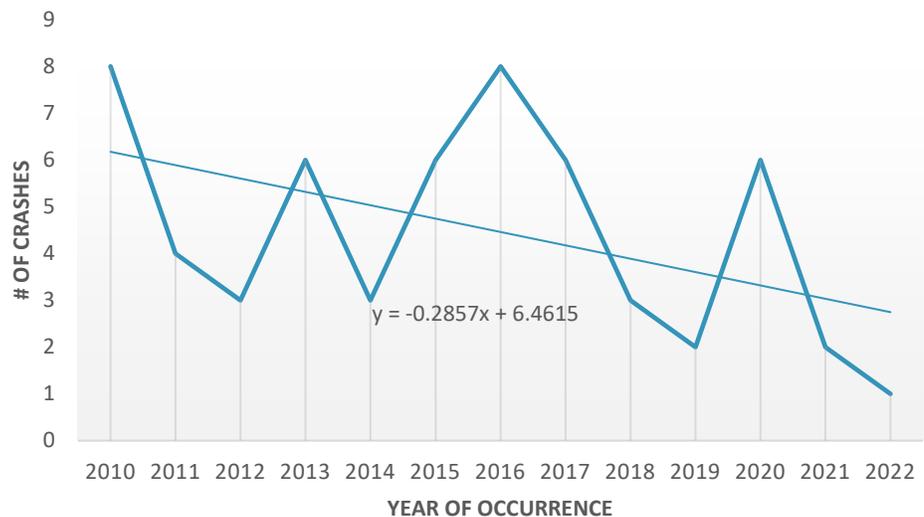
bike/ped related crashes annually.

However, the trend has been

**declining.**

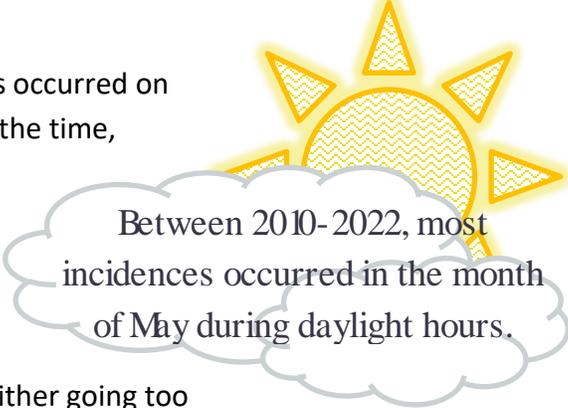
Frequency of Bike/Ped-Related Crashes By Year

City of Hudson, WI



In total, there were 58 bicycle and pedestrian related crashes between January 2010 and April 2022. Details show that Hudson had an average of 4.8 of these incidences per year. However, the rate of crash occurrences has been slowly declining at a rate of 0.2857 crashes every year. Although it is great that the rate of these crashes is declining, this rate would continue to result in an average of 3-4 crashes annually in the next 10 years.

Further details show that approximately 74% of incidences occurred on streets with speed limits of 25 miles per hour, and 76% of the time, these incidences occurred at intersections. 45% of incidences included bike users while another 40% were pedestrians. The remaining 15% were documented in police reports as 'other' incidences. Lastly, between 2010 and 2022, 85% of incidences involved "inattentive driving" or "a failure to yield", indicating that drivers are either going too fast for conditions and/or are distracted while driving.



Between 2010-2022, most incidences occurred in the month of May during daylight hours.

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## Public Engagement

This plan was developed with assistance from the City of Hudson’s Bicycle and Pedestrian Advisory Committee along with several community participants. The process began in April 2019 and started with outlining a public outreach approach which included the mixing of in-person and online interactions with the community. With the use of pop-up stands at community events and activity centers, along with a POLCO survey and web-based GIS mapping application, the plan had garnered a total of 410 responses across the various outreach activities.

The [Online Mapping Exercise](#) and [Pop-Up Booth](#) were both comprised of the same activity held across two types of platforms: virtual and in-person. The exercise involved residents placing ‘dot markers’ in specific locations in the City that they would like to see particular improvements, such as new trail connections, improved crosswalks, lighting, pedestrian amenities, etc. The Pop-Up Booth, held from December 16<sup>th</sup>-19<sup>th</sup>, 2019 at the local YMCA in Hudson, and also held at the Hudson Hot Air Affair Festival in February 2020, acquired 166 total comments from community members. Of these comments, 123 were requests for new trail connections. In addition to the Pop-Up Booth, an additional 87 comments were received by community members through the Online Mapping application between September 1<sup>st</sup>, 2019 – December 31<sup>st</sup>, 2019. Mirroring concerns shown by residents at the pop-up booth, 56 of the 87 online comments were requests for new trail connections. Results of this exercise are depicted in Appendix A-3.

A [Hudson Bicycle and Pedestrian Survey](#) was performed through the online platform POLCO and was online between March 10, 2020 and April 15<sup>th</sup>, 2020. This survey acquired 157 total respondents. Questions asked in the survey along with results are provided in Appendix A-1.

Additional outreach was acquired during the [2019-2020 Comprehensive Plan Community Survey](#), where residents were asked a series of questions regarding the existing trail and sidewalk systems in Hudson and whether they agreed or disagreed with the following statements:

- ❖ “The existing trail system is adequate.”
- ❖ “The off-street trail system should be improved/extended.”
- ❖ “The existing sidewalk system is adequate.”

309 community surveys were completed (5.1% of all 5,909 households in 2019), and the results indicated that only 22% of respondents feel the existing trail system is adequate, and only 31% feel the existing sidewalk system is adequate. Detailed responses are provided in Appendix A-2. These numbers indicate that there are gaps in the City’s trail and sidewalk systems that leave the current system undesirable. Making the system more accessible and exciting will be a key component to the success of this plan.

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The Bicycle and Pedestrian Advisory Committee then held meetings to discuss the vision and goals of the plan, utilizing key talking points that were received through community feedback from the pop-up events and the online surveys.

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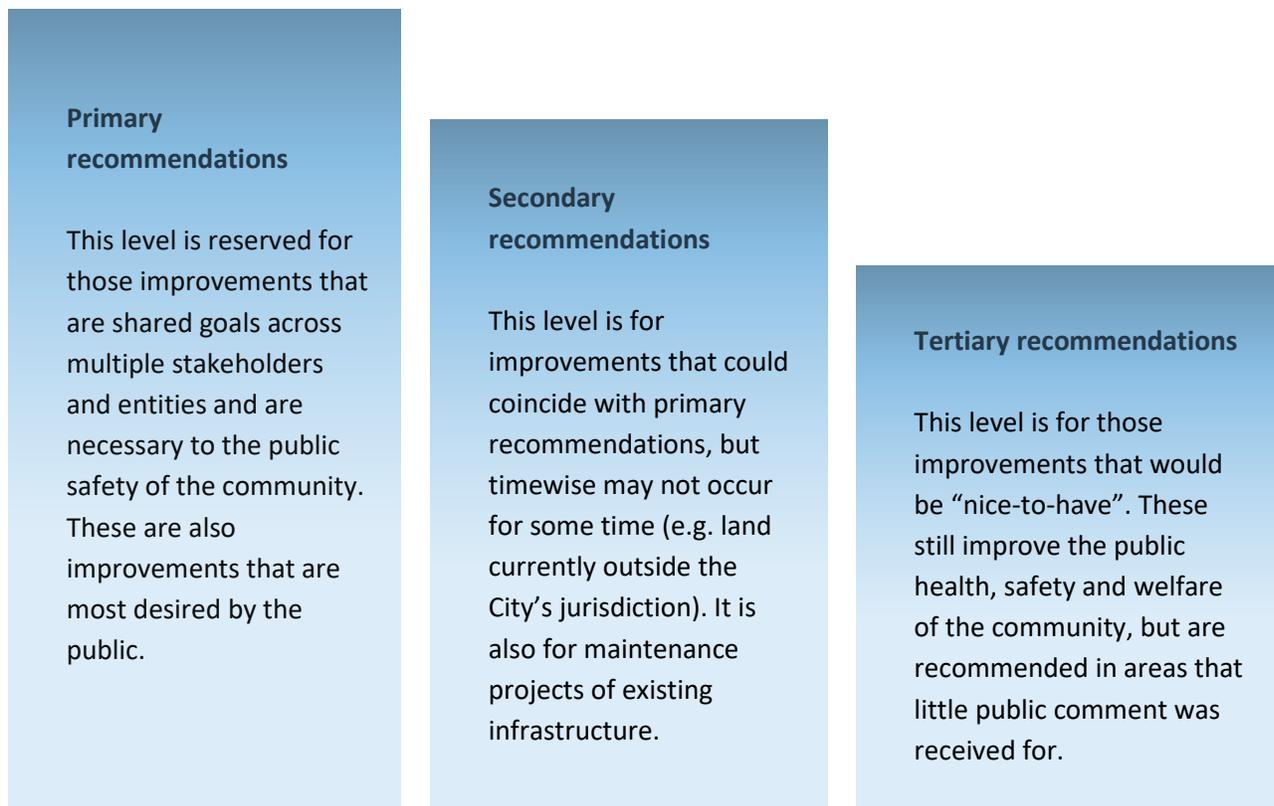
# Recommendations

Based on the review of existing conditions, public input, and analysis of the existing network, recommendations to improve bicycling and walking conditions in the City of Hudson were developed. In addition to facility recommendations, this section also outlines additional policies to be implemented and education, encouragement, enforcement, and evaluation programs to encourage their use.

## Network and Facility Recommendations

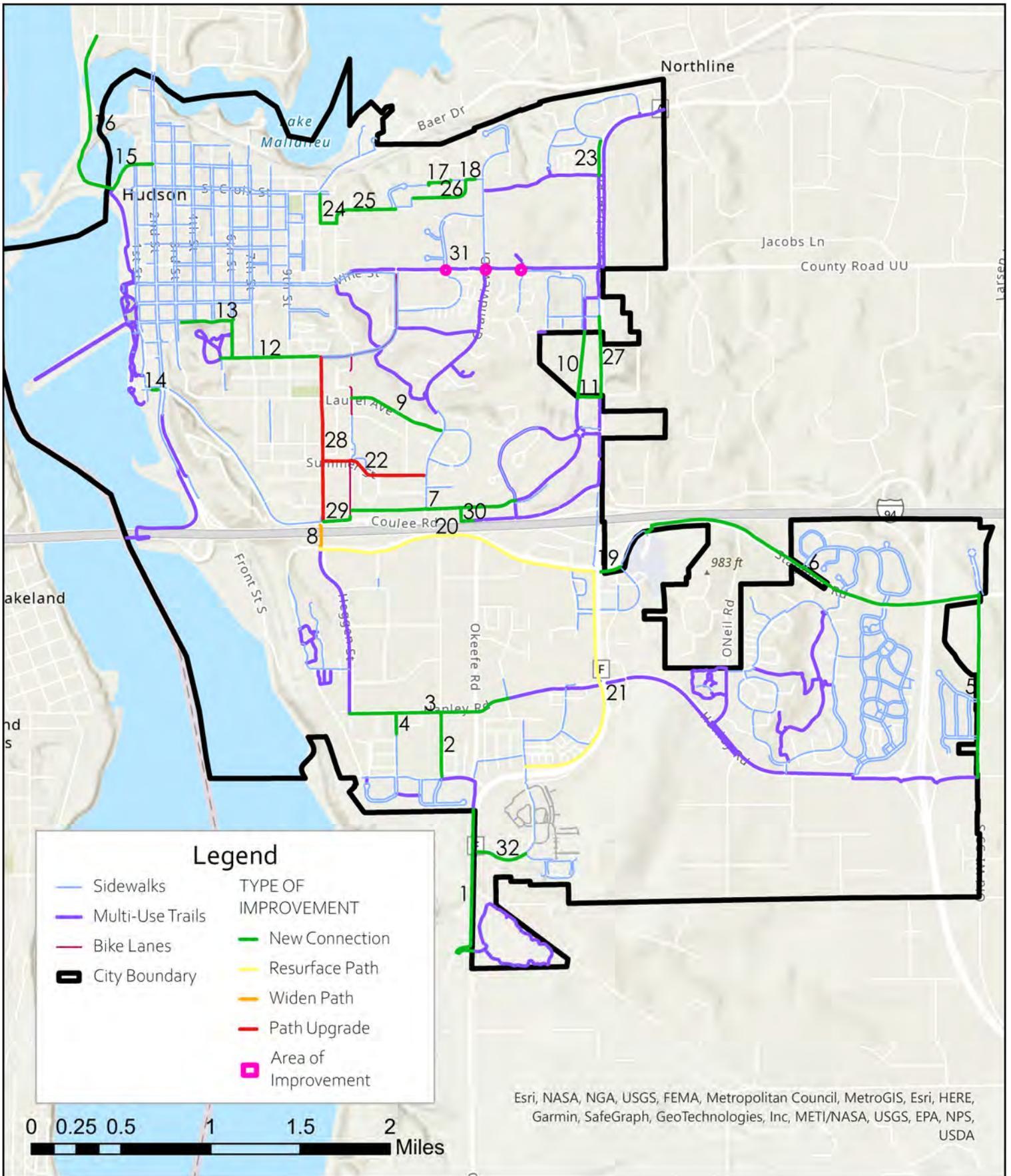
When complete, the City of Hudson Bicycle and Pedestrian recommendation network will consist of over 9 miles of bike lanes, trails, and sidewalk improvements linking Hudson residents to schools, parks, employment opportunities, cultural amenities, and institutions. The following is a map and table of listed recommended facility extensions and additions. Further details of each improvement are also described. Because these recommendations are conceptual, however, some recommendations may change as individual projects are designed and implemented.

This master plan organizes facility recommendations into a three-tier system:



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The three-tier system is meant to be a tool for city staff and advisory committee members to utilize in 'grading' specific network improvements against each other in terms of public health and safety attainment and over-arching goals with other plans. In no way shall this system be used as a means of determining the time when such network improvements are to be completed. When or if situations arise in the future that such improvements can be completed simultaneously with other construction projects, or grant funding is attained that can be applied to improvements in any of the three tiers, the city shall intend to make full use of such opportunities.



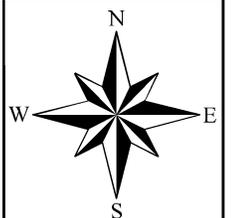
City Hall  
505 3rd Street  
Hudson WI 54016

Map (c) City of Hudson  
Data (c) City of Hudson, & ESRI (base map)

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# Proposed Bike Ped Improvements

June 2022  
Hudson, Wisconsin



Map ID #	Location	Improvement Type	Tier Rank	# Rank	Length (mi)
1	<b>County Road F</b> <i>(southbound to River Crest)</i>	<b>New Connection</b> <i>(separated trail)</i>	Primary	2	0.67
2	<b>Industrial Street</b> <i>(between Hanley and Mayer)</i>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	17	0.25
3	<b>Hanley Road</b> <i>(between Heggen and Hanley)</i>	<b>New Connection</b> <i>(separated trail)</i>	Primary	7	0.64
4	<b>Aspen Drive</b> <i>(between Mayer and Hanley)</i>	<b>New Connection</b> <i>(sidewalk extension)</i>	Tertiary	30	0.08
5	<b>Old Highway 35</b> <i>(between Stageline and Hanley)</i>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	22	0.72
6	<b>Stageline Road</b> <i>(between roundabout and Old Hwy 35)</i>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	18	1.44
7	<b>Ward Avenue</b> <i>(between 13<sup>th</sup> and Hillcrest)</i>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	10	0.65
8	<b>Eleventh Street</b> <i>(between Coulee and Crest View)</i>	<b>Widen Path</b> <i>(expand from 5-ft to 7-ft)</i>	Primary	1	0.09
9	<b>Laurel Avenue</b> <i>(between 13<sup>th</sup> and 17<sup>th</sup>/Grandview)</i>	<b>New Connection</b> <i>(bike lane)</i>	Tertiary	25	0.38
10	<b>Annabelle Way</b> <i>(through Gagnon Property)</i>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	21	0.26
11	<b>Between Annabelle and Carmichael</b>	<b>New Connection</b> <i>(separated trail)</i>	Tertiary	32	0.09
12	<b>Wisconsin Street</b> <i>(between Prospect Park and 11<sup>th</sup>)</i>	<b>New Connection</b> <i>(bike lane with sidewalk; or trail)</i>	Secondary	12	0.38
13	<b>Walnut Street</b> <i>(between 3<sup>rd</sup> and 6<sup>th</sup>)</i>	<b>New Connection</b> <i>(rehab one-way road into trail extension down to Wisconsin)</i>	Secondary	15	0.36
14	<b>Buckeye Street</b> <i>(between 1<sup>st</sup> and 2<sup>nd</sup>)</i>	<b>New Connection</b> <i>(sidewalk extension)</i>	Primary	6	0.02
15	<b>Xcel Energy Property</b> <i>(between St Croix and River)</i>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	19	0.21
16	<b>Train Tracks</b>	<b>New Connection</b> <i>(separated trail)</i>	Secondary	20	0.71
17	<b>Willow Drive</b> <i>(between Birch and path to Chestnut)</i>	<b>New Connection</b> <i>(sidewalk extension)</i>	Tertiary	29	0.10
18	<b>Ash Drive</b> <i>(between Chestnut and Grandview)</i>	<b>New Connection</b> <i>(sidewalk extension)</i>	Tertiary	28	0.04
19	<b>Stageline Road</b> <i>(between Carmichael and Center)</i>	<b>New Connection</b> <i>(trail extension)</i>	Secondary	14	0.08
20	<b>Crest View Drive</b> <i>(between Heggen and Carmichael)</i>	<b>Resurface Path</b>	Secondary	16	1.12
21	<b>Carmichael Road</b> <i>(between Crest View and Albert)</i>	<b>Resurface Path</b>	Secondary	13	0.99

22	<b>Summer Street</b> <i>(between 11th and 17th)</i>	<b>New Connection</b> <i>(sidewalk)</i>	Primary	5	0.42
23	<b>Carmichael Road</b> <i>(between County Trail and Burl Oak)</i>	<b>New Connection</b> <i>(sidewalk)</i>	Primary	4	0.14
24	<b>Eleventh Street</b> <i>(between St Croix and Fillmore)</i>	<b>New Connection</b> <i>(sidewalk)</i>	Primary	8	0.12
25	<b>Oak Street</b> <i>(between 11<sup>th</sup> and Juniper)</i>	<b>New Connection</b> <i>(sidewalk)</i>	Primary	9	0.33
26	<b>Chestnut</b> <i>(between Ash and Maple)</i>	<b>New Connection</b> <i>(sidewalk)</i>	Secondary	26	0.26
27	<b>Carmichael Road</b> <i>(between Sofia and Deer Haven)</i>	<b>New Connection</b> <i>(trail extension)</i>	Secondary	11	0.32
28	<b>Eleventh Street</b> <i>(between Wisconsin and Coulee)</i>	<b>Path Upgrade</b> <i>(upgrade bike lane to separated multi-use trail)</i>	Primary	3	0.65
29	<b>Coulee Road</b> <i>(between 11<sup>th</sup> and 13<sup>th</sup> S)</i>	<b>New Connection</b> <i>(separated trail)</i>	Tertiary	27	0.12
30	<b>Coulee Road</b> <i>(between Coulee and 19<sup>th</sup>)</i>	<b>New Connection</b> <i>(trail extension)</i>	Secondary	24	0.10
31	<b>Intersections of Vine and Spruce, Grandview, and Diamond</b>	<b>Intersection Improvement</b>	Tertiary	31	n/a
32	<b>Brecken Drive</b> <i>(between Sharon and County Rd F)</i>	<b>New Connection</b> <i>(sidewalk)</i>	Secondary	23	0.20
<b>TOTAL</b>					<b>11.94</b>

## Improvement 1. County Road F.



Across many stakeholders, a primary concern within the community is safe routes to school. Connecting all schools with bicycle and pedestrian infrastructure would allow the City of Hudson’s youth to transport themselves in a healthy and engaging manner. In turn, providing this alternative transportation method could have the potential of unburdening the school district’s bus system, a common concern amongst parents in survey results from the 2018 Safe Routes to School Plan.

This improvement is recommended as a **separated paved multi-use trail** due to the higher speed limit along County Road F. The trail is proposed to extend through the existing YMCA Camp St Croix property and connect southbound to River Crest Elementary School through the existing tunnel beneath County Road F. In total, the length of this improvement is approximately 0.67 miles. As this connection is a shared goal between city residents, the school district, and St Croix County, and has a LOTS ranking of 3-4, this improvement is rated: **PRIMARY**.

### LEGEND

-  City Boundary
-  Sidewalks
-  Multi-Use Trails
-  New Connection

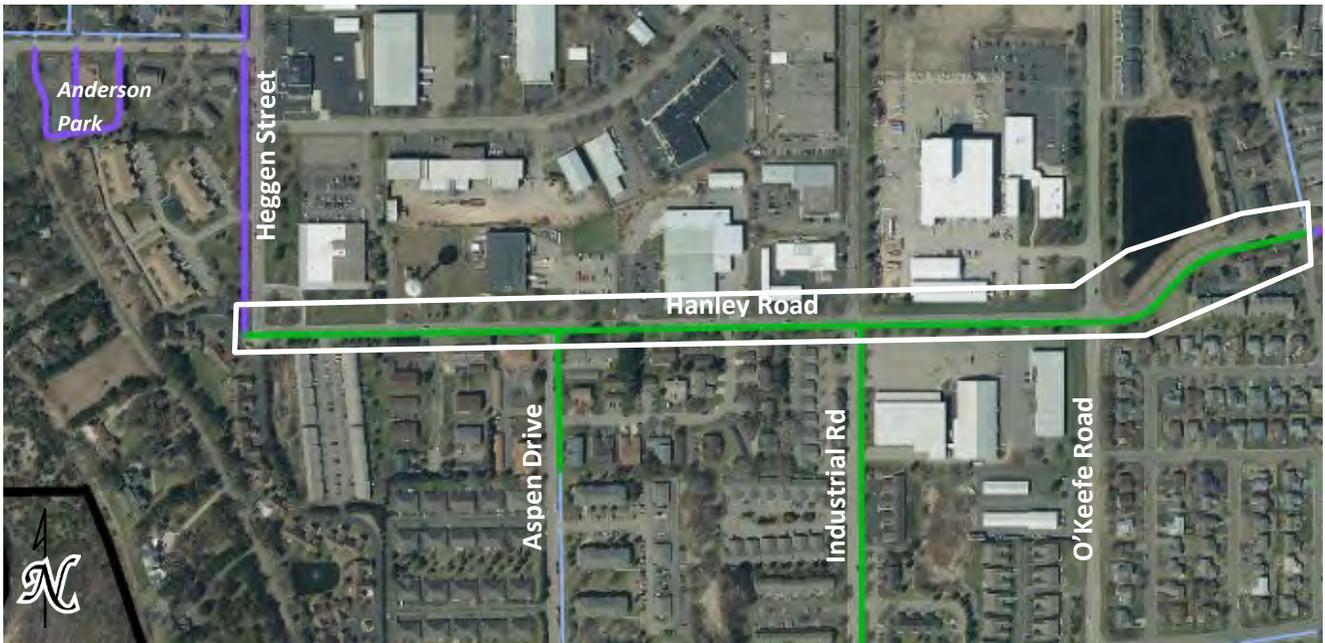
## Improvement 2. Industrial Street.



As this area is one of the City of Hudson’s densest residential neighborhoods, complete with several twin homes and multi-family apartments, a trail along industrial that connects up to Hanley would serve as a great connection for an even greater number of people. The street’s LOTS ranking falls within a 3-4, indicating it is not friendly for the average bicyclist. The total length of this improvement would be approximately 0.25 miles. As this road has less traffic compared to its counterpart on O’Keefe and has a lower speed limit (25 mph compared to 35 mph), this improvement is recommended as a [separate paved multi-use trail](#). This improvement is rated: **SECONDARY**.

LEGEND	
	Sidewalks
	Multi-Use Trails
	New Connection

### Improvement 3. Hanley Road.



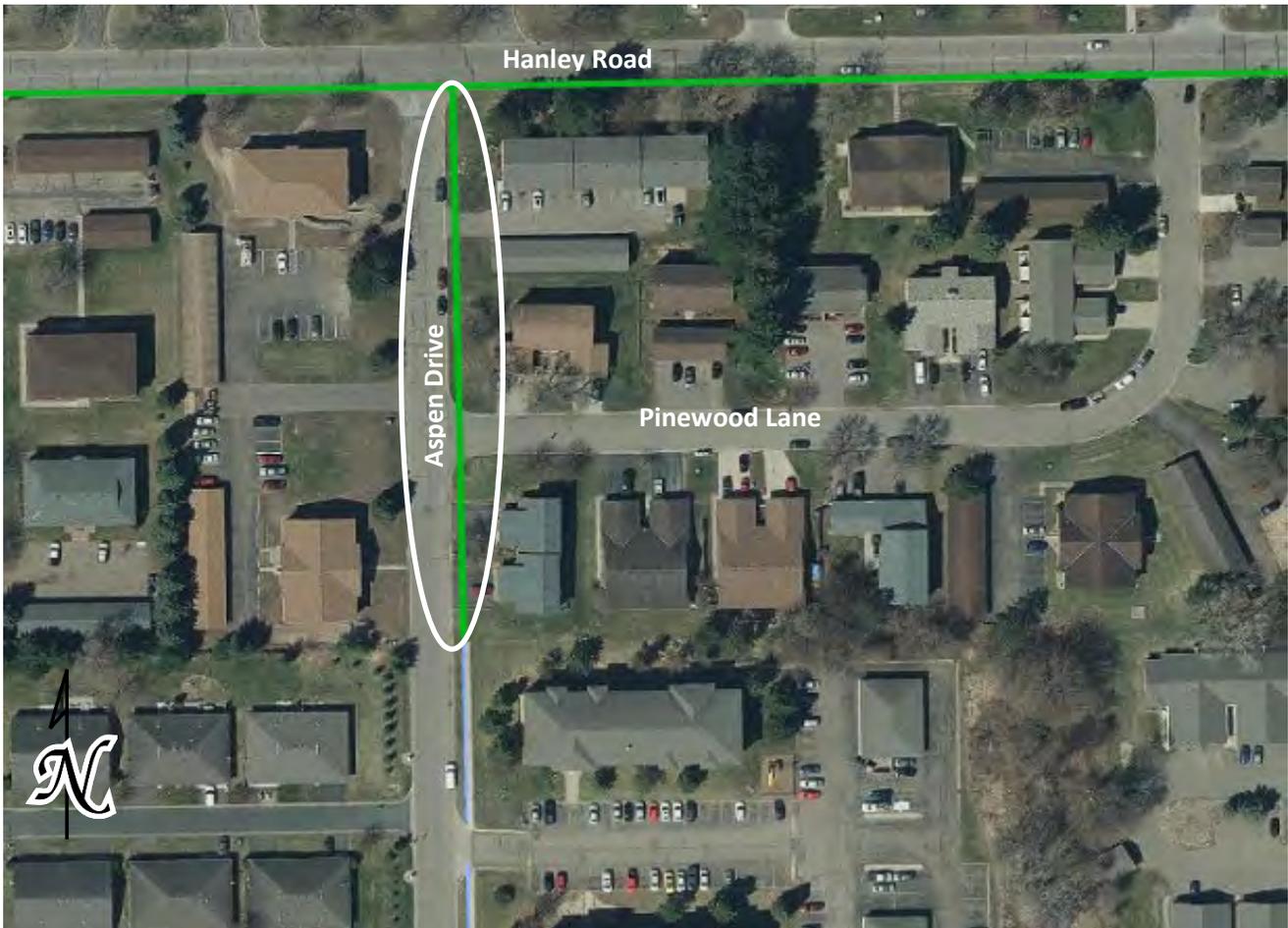
#### LEGEND

-  City Boundary
-  Sidewalks
-  Multi-Use Trails
-  New Connection

This improvement is proposed to be a **separated paved multi-use trail** on the south side of Hanley Road, adjacent to residential uses rather than the industrial businesses along the north. Being located along the south would allow the pathway to connect to other sidewalk and trail proposals and existing facilities without forcing users to cross a busy street. The proposed length of this segment is approximately 0.64 miles long, connecting from the existing Heggen Street multi-use trail eastward to the existing Hanley Road multi-use trail, completing the loop. Several residents noted this as a necessary connection as it would allow residents

living on the southside of the city access to Anderson Park, and it would likewise provide a safe pathway to walk or bike to the Aldi's grocery store (also located along Hanley Road, further eastward than what is shown in the map above). At the intersection of Hanley and O'Keefe, a resident also requested for a stop light to be installed. Due to these reasons, and the roadway's LOTS ranking of 3-4, this improvement is rated: **PRIMARY**.

### Improvement 4. Aspen Drive.



This improvement is proposed to be a **sidewalk extension** along Aspen Drive connecting from the existing sidewalk on the east side up to Hanley Road where the proposed multi-use trail improvement would be located. This extension would be approximately 0.08 miles in length. No specific comments from the public were received pertaining to this proposed project, therefore this improvement is rated: **TERTIARY**.

LEGEND	
	Sidewalks
	New Connection

## Improvement 5. Old Highway 35.



The location of this proposal is seen as critically important both for avid bike riders as well as those in the Hudson Meadows neighborhood to connect to Hanley and Stageline Road, two major corridors in the City of Hudson. With the movie theater development and the Hudson Meadows neighborhood all located east of the current Highway 35 (immediately west of what is shown in the map), this area of the city is an island which is only really accessible by car. Street conditions are unsafe for residents to walk or bike on these streets given the higher speed limits and lack of infrastructure.

St Croix County, in its 2017 Bicycle and Pedestrian Plan, anticipated this route as including only minor improvements such as wayfinding signage and strategically placed street markings such as bike lanes or sharrows. However, to make this a safer and more accessible route for all user types, it is recommended that this route include a **separated paved multi-use trail**. The length of the proposal would be approximately 0.72 miles. As this route was highlighted by several residents as a necessary route addition but the street and public right of way is all currently maintained by the Town of Hudson, it is rated: **SECONDARY**.

### LEGEND

- City Boundary
- Sidewalks
- Multi-Use Trails
- New Connection



### Improvement 6. Stageline Road.

Stageline Road is a major corridor in the City of Hudson as it helps to connect the east side of the city with the “Hill” business district. Likewise, it also provides access to the hospital.

Many comments were received from city residents desiring a connection through this corridor. Safety concerns were noted from residents attempting to walk along this street without facilities in place while vehicles speed through this corridor, typically above the speed limit of 35 miles per hour.

Other concerns were noted as to the safety and efficacy of residents crossing Stageline Road between north and south. An incident was reported to have occurred at the intersection of Stageline Road and Heritage Boulevard, where the roadway is 6-lanes in width with no pedestrian islands in place for safe crossing.

This recommendation is extensive and includes **road-dieting**, to encourage drivers to slow down, **HAWK signals or rapid flashing beacons** for pedestrians and bicyclists to cross between north and south, and a **separated sidewalk** connecting from the roundabout in front of the hospital eastbound to the movie theater at Old Highway 35. The length of this improvement is proposed to be 1.44 miles and is located in both the City of Hudson and the Town of Hudson. Due to this, and the roadways LOTS ranking of 3-4, this improvement is rated: **SECONDARY**.

## LEGEND

- City Boundary
- Sidewalks
- Multi-Use Trails
- New Connection

## Improvement 7. Ward Avenue.



This improvement would include a 0.65-mile **multi-use trail** along the north side of Ward Avenue between 13<sup>th</sup> Street and Hillcrest Drive and would serve as a connection corridor on a side street of Coulee Road. Coulee is a very busy street with higher speed limits (35mph) than Ward Avenue (25mph) and contains a large number of commercial driveways, therefore it is deemed safer and more appealing to have a bicycle and pedestrian connection along Ward rather than continuing down Coulee Road where a trail extension would remove patches of grass and make the area less desirable by taking on the appearance of a concrete desert.

As several comments were received from the public on a desire to create a connection between 13<sup>th</sup> Street and Hillcrest, and with this roadway being ranked a LOTS 3-4 as very unfriendly for the average person riding a bicycle, this improvement is rated: **SECONDARY**.

LEGEND	
	Sidewalks
	Multi-Use Trails
	Bike Lanes
	New Connection

## Improvement 8. Eleventh Street.



This improvement crosses between Coulee Road and Crest View Drive along the 11th Street Bridge (also referred to as the Heggen Street Bridge). There have been multiple requests within the community to have this bridge widened due to incredible traffic backups, but there has also been a lot of feedback received through this plan’s public outreach regarding pedestrian connections along the bridge and a desire to widen the existing sidewalk along the west side of the bridge. Currently the sidewalk width is approximately 5-6 feet wide, meeting the bare minimum requirements for a sidewalk. However, to allow bicyclists along this route, the sidewalk should be **widened into a multi-use path preferably 10-12 feet wide**. This improvement would require the City of Hudson to work with the Wisconsin Department of Transportation as this is a bridge crossing over Interstate 94.

In line with this improvement, it is also recommended that the intersection of Crest View Drive and 11th Street/Heggen Street be made **more ADA accessible** as there have been past reported problems for wheelchair users to access pushbuttons at the lights (i.e. wheelchairs getting stuck in the grass/mud).

This path connection is a shared goal between the City of Hudson and St Croix County and is ranked LOTS 2 as an unfriendly bike route for children; therefore, this improvement is rated: **PRIMARY**.

### LEGEND

-  City Boundary
-  Sidewalks
-  Multi-Use Trails
-  Bike Lanes
-  New Connection
-  Resurface Path
-  Widen Path
-  Path Upgrade



**ADA inaccessible pushbutton at SW corner of Crest View Drive and Heggen Street intersection**

### Improvement 9. Laurel Avenue.



During public outreach sessions, many comments were received from the community regarding a new connection through Laurel Avenue, most particularly due to its proximity to EP Rock Elementary School as well as nearby bike lanes and trails. This improvement would recommend a **bike lane** along Laurel’s north side and would be approximately 0.38 miles in length. As this improvement was not identified in other recent community or school district plans, it is rated: **TERTIARY**.

### Improvement 10. Annabelle Way.



This improvement has been identified as a future **multi-use trail** corridor and has been planned as such by city staff. The intention is for this to finish the full route along Annabelle Way from the Hillcrest Drive roundabout up north to Vine Street, allowing an easy connection for students to walk or bike to the Middle School. Being that the property in question is currently within the Town of Hudson’s jurisdiction, this improvement is rated: **SECONDARY**. It is further suggested at the time of development that the developer shall be required to install this multi-use trail facility and complete the connection through a development agreement.

## LEGEND

- City Boundary
- Sidewalks
- Multi-Use Trails
- Bike Lanes
- New Connection

### Improvement 11. Between Annabelle and Carmichael.



As a potential improvement option, a **multi-use trail** connection 0.09 miles in length from Annabelle Way over to Carmichael Road at this location would allow bicyclists and pedestrians an alternative path that avoids multiple business driveways along Carmichael Road. However, being that this improvement was not mentioned by the public, nor mentioned in any other plans and is only seen as an option, it is rated: **TERTIARY**.

### Improvement 12. Wisconsin Street.

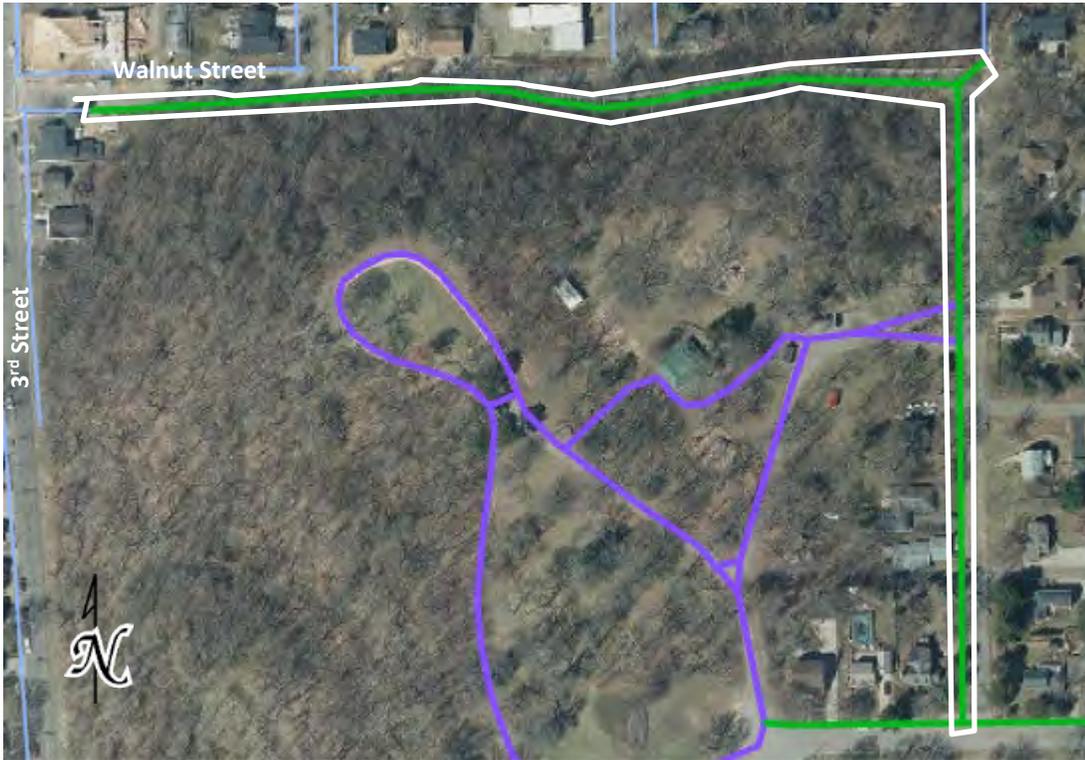


Wisconsin Street is a major connection between the High School and Prospect Park. In the past, public works and engineering were able to construct sidewalks and bike lanes along half the length of Wisconsin. However, the trails end at 11th Street. Many residents spoke about desiring this corridor to be completed, extending further westward toward Prospect Park. This improvement is proposed to be a **bike lane and sidewalks** (possibly a multi-use trail) along the north side of Wisconsin to serve as an extension of the existing bike lane infrastructure and would extend approximately 0.38 miles between Prospect Park/Blakeman Boulevard and 11<sup>th</sup> Street. This improvement is rated: **SECONDARY**.

## LEGEND

- City Boundary
- Sidewalks
- Multi-Use Trails
- Bike Lanes
- New Connection
- Path Upgrade

### Improvement 13. Walnut Street.



**LEGEND**

- City Boundary
- Sidewalks
- Multi-Use Trails
- New Connection

Public comments reflected a great desire to reconnect Prospect Park with downtown. In the past, there was a staircase connecting the park to 3rd Street. However, the stairs were in great disrepair, and so to prevent injuries, the city removed them. In place of a steep staircase, a more ADA-accessible alternative would be a multi-use trail improvement utilizing Walnut’s one-way roadway leading from 6<sup>th</sup> Street to 3<sup>rd</sup> Street. Closing off the one-way road from vehicular use would make it safe for all users. This improvement would be approximately 0.36 miles in length and is rated: **SECONDARY**.

## Improvement 14. Buckeye Street.



### LEGEND

-  City Boundary
-  Sidewalks
-  New Connection

As part of the Lakefront Park corridor, many residents, particularly bicyclists wanting to ride over to Minnesota, requested for this segment of sidewalk to be filled in. This section of road is ranked a LOTS 2, making it not as friendly as the rest of the Lakefront corridor for bicyclists. To fill in this gap of 0.02 miles, this recommendation includes a **shortening of driveway widths** along with an **ADA-accessible sidewalk extension**. As this segment has been noted in the St Croix Bicycle

and Pedestrian Plan as a location in need of Bicycle Enhancements, this improvement is therefore rated: **PRIMARY**.

## Improvement 15. Xcel Energy Property.



### LEGEND

-  City Boundary
-  Sidewalks
-  Multi-Use Trails
-  New Connection

Discussion between the City of Hudson and Xcel Energy occurred in the past regarding a possible trail connection through Xcel's property up to River Street from where Lakefront Park Trail currently ends at St Croix. An agreement was drafted but never signed by the City of Hudson, however, as one of the goals of this plan is to create a viable bicycle trail connection toward North Hudson, this option should be considered. A **paved multi-use trail** through the Xcel property would constitute 0.21 miles in length.

In addition to the history noted above, many residents provided comment on a desire for a connection to North Hudson. Opposition from neighborhood residents, however, was also voiced at the time when Xcel was cleaning the site of debris. Due to this, this improvement is ranked:

**SECONDARY.**

## Improvement 16. Train Tracks to North Hudson.



This improvement, perhaps the most favored amongst residents who provided feedback for this plan, would be an alternative route to North Hudson separate from the previous Improvement 15, connecting scenically along the St Croix River.

Although most favored, this would also be the most difficult to achieve as it would require several entities (Union Pacific Railroad, St Croix County, City of Hudson, and Village of North Hudson) to work together on the implementation of this connection.

This improvement would require a **paved multi-use trail** along with a **bridge upgrade** at the historic Lake Mallalieu dam. The pathway would extend 0.71 miles from the City of Hudson’s St Croix Street, run along part of the abandoned Union Pacific railroad, cross the Lake Mallalieu dam, and end at Wisconsin Street N in the Village of North Hudson.

Due to the challenges involved with this improvement, it is ranked: **SECONDARY**.

### LEGEND

- City Boundary
- Sidewalks
- Multi-Use Trails
- New Connection

### Improvement 17. Willow Drive.

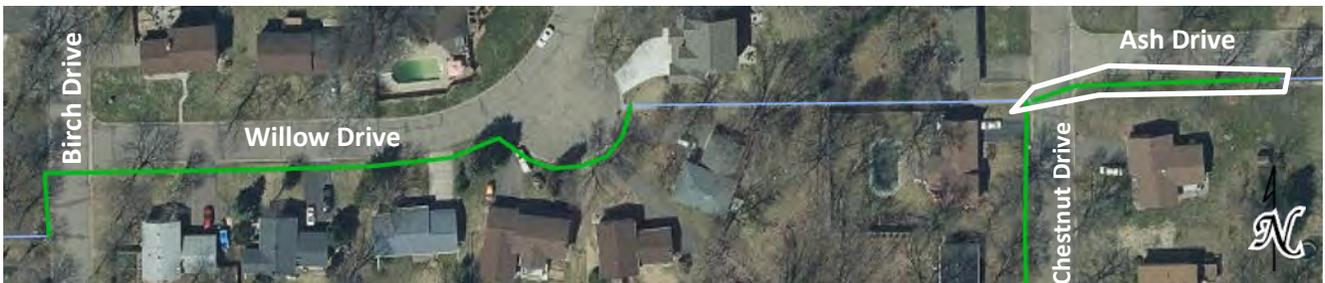


To create a more accessible community, this will involve updating the current infrastructure. Perhaps one unknown “trail” is that which goes through the Woodland Neighborhood, north of the High School. Years ago, planners of the City of Hudson required pedestrian connections through the neighborhood. Kids certainly use this trail to get to school.

However, it is not ADA-accessible for all kids to use. To welcome all users, **accessibility ramps** are proposed for this improvement along with a **sidewalk extension** between Willow Drive and Chestnut Drive; a length of approximately 0.1 miles. As this was not a voiced concern during public outreach events, this improvement is ranked: **TERTIARY**.

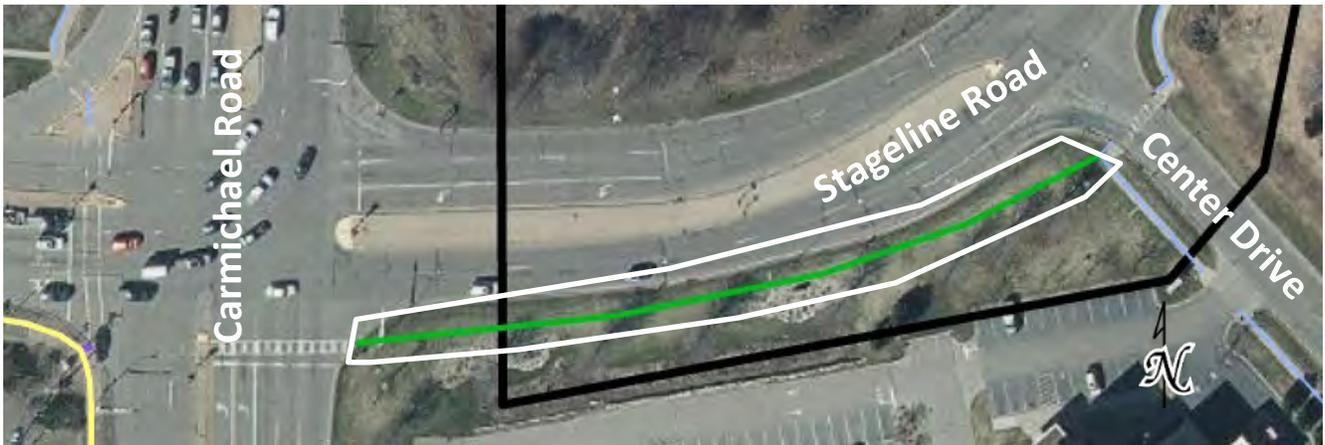
LEGEND	
	Sidewalks
	New Connection

### Improvement 18. Ash Drive.



As part two to Improvement 17, this improvement is a further **sidewalk extension** and **accessibility ramp installation** consisting of 0.04 miles at the property of 1912 Chestnut Drive. As no public comments were received for this improvement, it is ranked: **TERTIARY**.

### Improvement 19. Stagline Road.



**LEGEND**

-  City Boundary
-  Sidewalks
-  Multi-Use Trails
-  New Connection
-  Resurface Path
-  Widen Path

As part of the Stagline Road connections, this improvement would further connect the roadway to Carmichael Road. Currently the intersection of Carmichael and Stagline has a crosswalk that leads to nowhere. With no facilities in place, this leaves little incentive for pedestrians or bicyclists to traverse this area of the city. A comment was received from the public regarding the difficulty of using this intersection. This improvement is proposed as a 0.08-mile **trail extension** along the south of Stagline Road between Carmichael Road and Center Drive. With a LOTS ranking of 3-4 and a location mostly outside of the city’s jurisdiction, this improvement is ranked: **SECONDARY**.

### Improvement 20. Crest View Drive.



Public feedback from avid bicyclists indicated a strong desire for the Crest View Drive **multi-use trail to be repaved**. The trail extends from Heggen Street eastward to Carmichael Road and lies along the road’s south side. The length of this trail is approximately 1.12 miles. As this is a major corridor and has a LOTS ranking of primarily 2 (a segment between Industrial and O’Keefe is ranked 3-4), this improvement is ranked: **SECONDARY**.

## Improvement 21. Carmichael Road.



The existing trail along Carmichael Road between Crest View Drive and Albert Street was noted by local bicyclists as in need of repair. This improvement, therefore, involves repaving the Carmichael Road trail between Crest View Drive and Albert Street, along Carmichael's west side. This improvement is approximately 0.99 miles long and is ranked: **SECONDARY.**

### LEGEND

-  City Boundary
-  Sidewalks
-  Multi-Use Trails
-  New Connection
-  Resurface Path

## Improvement 22. Summer Street.



In the 2018 Safe Routes to School Plan, it was noted that snow storage along Summer Street causes issues for students walking to school in the winter. As snow piles up along the edge of the roadway, it edges kids further into the street, putting them in danger of vehicular traffic.

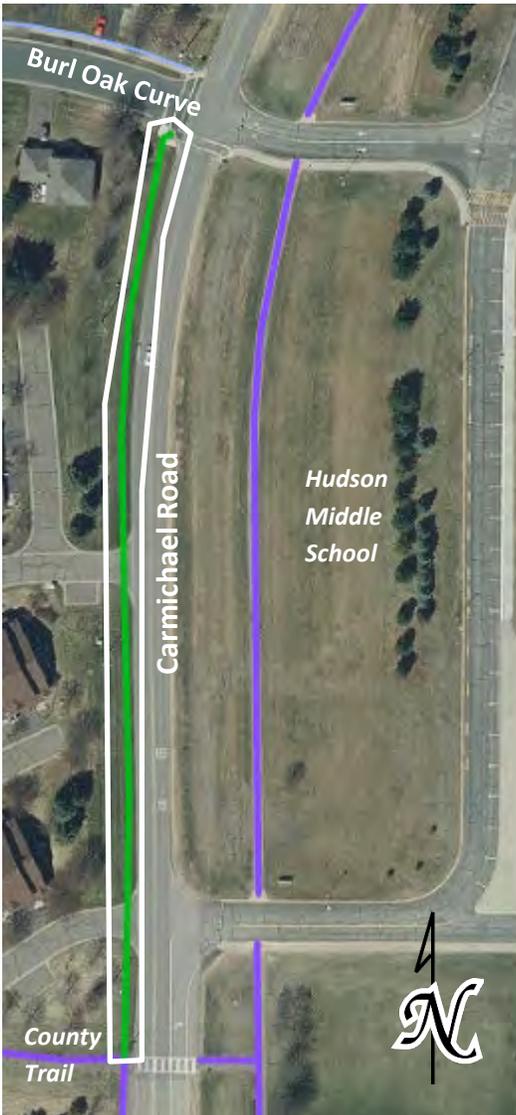
This improvement would upgrade the route from the existing on-street bike lane to an **off-street sidewalk** to keep kids walking to school separated safely from vehicles, especially in the winter. The length of this improvement is approximately 0.42 miles long and would also require **ADA-accessible curb ramps to be installed** along the length of the proposed improvement.

Due to the necessity of safety involved with this improvement, it is ranked: **PRIMARY**.

**LEGEND**

-  City Boundary
-  Sidewalks
-  Bike Lanes
-  Path Upgrade

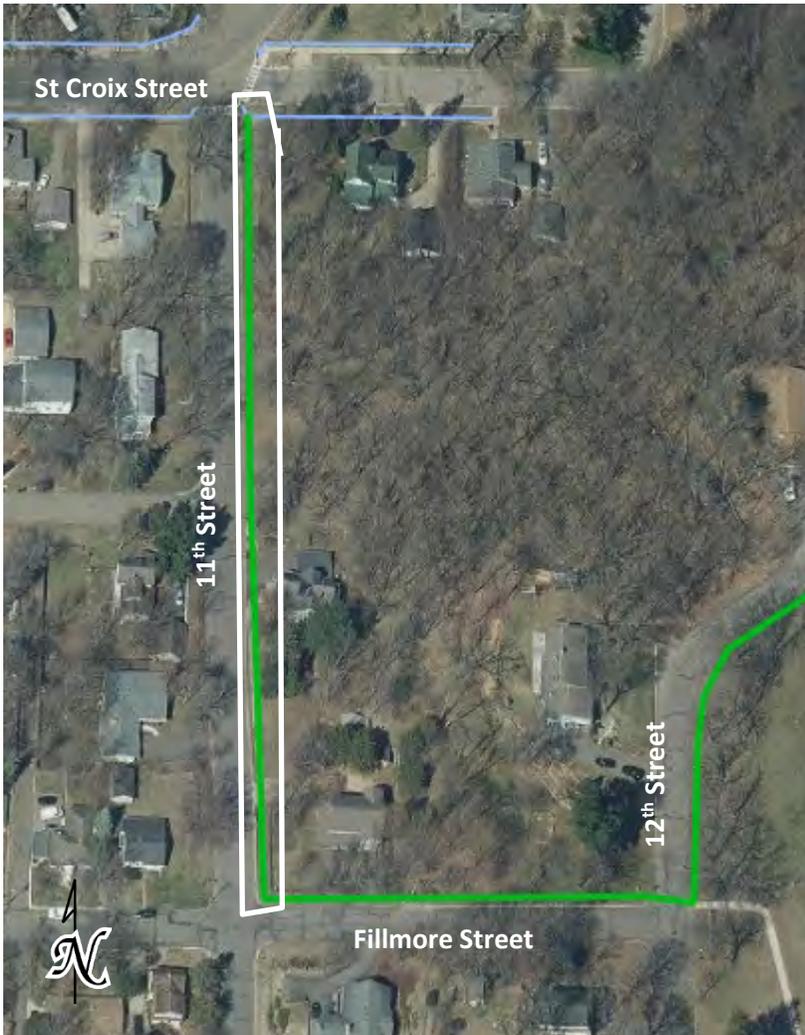
### Improvement 23. Carmichael Road.



Proposed in the Safe Routes to School 2018 plan, this recommendation proposes a **sidewalk extension** along the west side of Carmichael Road across the street from the Middle School and runs between the County Trail up to Burl Oak Curve. This improvement is approximately 0.14 miles long. Several public comments included a desire to connect sidewalks in this neighborhood as a means of improving overall safety for children walking to school. Therefore, this improvement is rated: **PRIMARY**.

LEGEND	
	New Connection
	Sidewalks
	Multi-Use Trails

### Improvement 24. Eleventh Street.



**LEGEND**

-  New Connection
-  Sidewalks
-  Multi-Use Trails

Both improvements 24 and 25 call for the installation of new **sidewalks** and harbor the same goal: to make safer connections for kids to reach the high school. City staff and Council members receive public comments annually regarding the safety of walking to the high school, particularly in regard to these streets that lie north of the high school. Improvement 24 calls for sidewalk along the east side of 11th Street, approximately 0.12 miles in length. Improvement 25 calls for sidewalk

along Fillmore Street’s north side, a crossing over to 12th Street’s east side, running northbound to Oak Street where the sidewalk would continue along Oak’s south side for approximately 0.33 miles. As these are Safe Routes to School oriented goals, they are both ranked: **PRIMARY**.

### Improvement 25. Oak Street.



## Improvement 26. Chestnut Street.



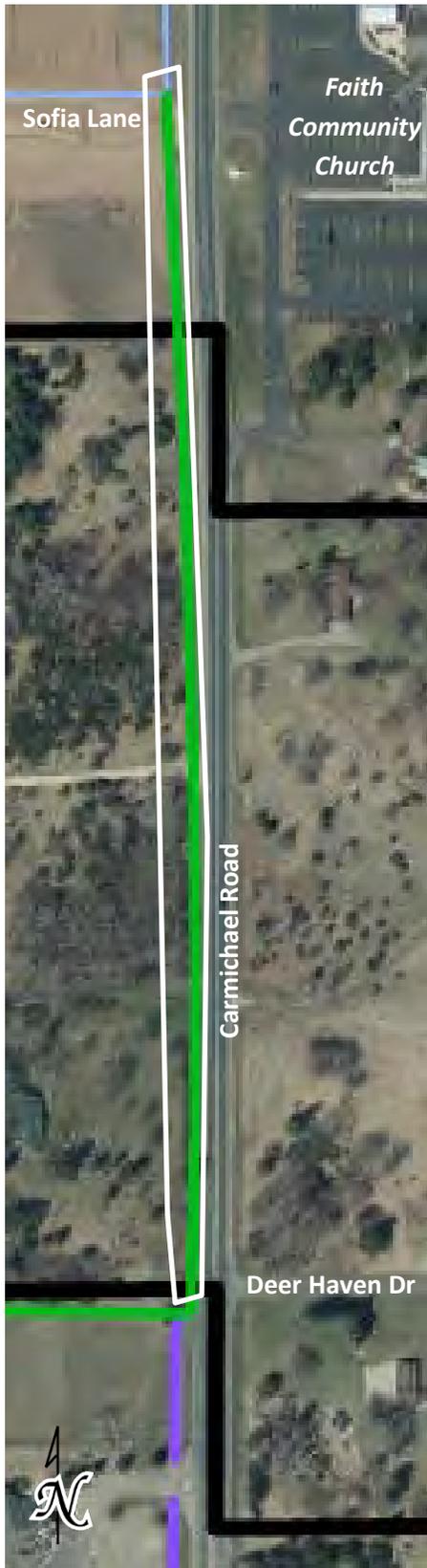
In relation to improvements 24 and 25, Chestnut Street is north of the Hudson High School as well and would provide additional Safe Routes to School connections in this neighborhood. This improvement would involve the construction of a new sidewalk along the north side of Chestnut Street between Ash Drive and Maple Drive, 0.26 miles in length. This improvement would be settled along a quaint residential street with several homes in the immediate vicinity, providing safe walking paths for students.

### LEGEND

-  New Connection
-  Sidewalks

Further, this improvement would also provide safe connections among residential homes in the neighborhood, allowing the area to feel more connected and provide the feeling of a welcoming community. Given the number of private driveways, however, this improvement may be difficult to implement. Due to these reasons, this improvement is ranked: **SECONDARY**.

## Improvement 27. Carmichael Road.



Perhaps the most popular improvement segment amongst local bicyclists according to the Mapping Exercise outreach method, improvement 27 consists of a **separated multi-use trail extension** between Sofia Lane and Deer Haven Drive along Carmichael Road's west side, extending approximately 0.32 miles in length.

Due to the fast speed limits and amount of traffic Carmichael Road handles daily, a separated trail along this route is highly encouraged for safety as well as enticing the public to use this route as average bicyclists and pedestrians tend to feel more comfortable along a trail that is more separated from speeding cars.

As much of this route is outside of city limits at the time of this plan's creation, this route is ranked: **SECONDARY**. At the time when, or if, the property adjacent to this right-of-way is annexed into the City for future development, any future platting and plan reviews should certainly include this bicycle and pedestrian route extension.

LEGEND	
	New Connection
	Sidewalks
	Multi-Use Trails

## Improvement 28. Eleventh Street.



Currently, Eleventh Street serves as an on-road bike lane corridor. However, as noted in previous improvements, this leads to safety concerns in winter where residents may be pushed further into the street due to large amounts of snow build up. Further, many residents living along this road have mailboxes and garbage bins leaning into this route, making it hazardous in all other seasons as well.

This improvement, therefore, calls for an **upgrade to the existing facilities from a bike lane to a separated multi-use trail** between Wisconsin Street and Coulee Road approximately 0.65 miles in length. The trail would remain along Eleventh Street’s east side and would continue to serve as a major corridor for bicycle and pedestrian users to connect down to the 11<sup>th</sup> Street/Heggen Street Bridge that joins the north and south halves of Hudson. Due to traffic loads along this roadway, a separated trail is deemed a much safer option, therefore this improvement is ranked: **PRIMARY**.

### LEGEND

- Sidewalks
- Multi-Use Trails
- Bike Lanes
- New Connection
- Widen Path
- Path Upgrade

## Improvement 29. Coulee Road.



As part of a major transportation connector in the city, the intersection of Coulee Road and 11th Street serves as one of the gateways connecting Hudson's north and south halves (separated by interstate 94). Therefore, making it more accessible by those walking and bicycling, and not just easy for vehicles to traverse, is deemed important.

Presently, there is a bike lane along 13<sup>th</sup> Street that ends abruptly in front of a gas station driveway. There are also several car-centric businesses in this area that have their own separate driveways, making the construction of this connection between 11<sup>th</sup> Street and 13<sup>th</sup> Street S difficult. However, if city staff and business owners work together to limit the number of driveways, possibly creating access easements for properties where needed, and adding a 0.12-mile **separated trail connection** on this block along Coulee Road's north side, it may be possible. This improvement is ranked: **TERTIARY**.

### LEGEND

- Sidewalks
- Bike Lanes
- New Connection
- Widen Path
- Path Upgrade

### Improvement 30. Coulee Road.



Perhaps as part two to improvement 7 (separated trail along Ward Avenue), improvement 30 would help connect the existing multi-use trail along Coulee Road up to the new Ward Avenue trail proposal. This would include 0.10 miles of **separated paved multi-use trail** along part of Coulee Road's north side and along 19<sup>th</sup> Street's east side. This improvement is ranked: **SECONDARY** and may require driveway closures along 19<sup>th</sup> Street into the existing gas station to improve safety for future bicycle and pedestrian users.

LEGEND	
	Sidewalks
	New Connection

**Improvement 31. Intersections of Vine and Spruce, Grandview, and Diamond.**

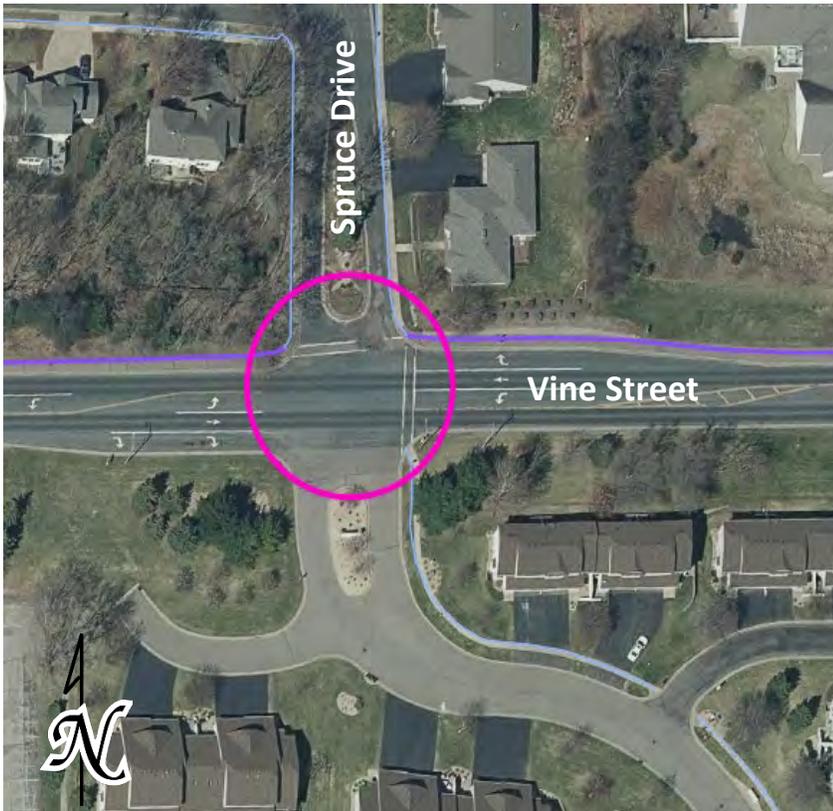


Image 1. Intersection of Spruce and Vine.

Resident feedback indicated these 4-lane intersections feel dangerous to cross. Therefore, **TERTIARY** rated suggestions are recommended:

- Extend the landscaped island entry southward at Spruce and Vine from the neighborhood to create a **pedestrian island**
- Redesign all three intersections to decrease the north/south crossing lengths for pedestrians, either by **road-dieting** or by constructing **pedestrian islands**.

### LEGEND

- Sidewalks
- Multi-Use Trails
- ◻ Area of Improvement



Image 2. Intersection of Diamond and Vine.



Image 3. Intersection of Grandview and Vine.

## Improvement 32. Brecken Drive.



The new St Croix Meadows development (commonly referred to by locals as the old dog track site), is being developed for residential, commercial, and recreational uses. Residential uses include single family homes, twin and row homes, as well as a future planned multi-family development. Recreational uses include a ballpark, brewery, and public park. There is also some planned commercial business spaces to be located within this development. As a potential hotspot for recreational activities amongst community members, making this area walkable is highly desired, especially given its proximity to River Crest Elementary to the south. This proposed route would include a new **sidewalk extension** along the south side of Brecken drive, between Sharon Lane and County Road F (approximately 0.2 miles in length). This improvement is rated **SECONDARY**.

LEGEND	
	City Boundary
	Sidewalks
	New Connection

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# Program Recommendations

Improvements to and continued support of education, enforcement and evaluation programs are critical to increasing the number of bicycle and pedestrian trips and safety. These programs can ensure that more residents know about new and improved facilities, learn the skills they need to integrate walking into their activities, and receive positive reinforcement about integrating walking into their daily lives. These recommended programs market the idea of walking to local residents and ensure a shift to walking as a transportation option. The following section presents program recommendations intended to support walking in the City.

## **Recommendation 1. Utilize Tactical Urbanism Programming**

Tactical Urbanism is a set of ideas, methods, and tools that help city governments and community groups work to build safer, happier, and more connected neighborhoods. These goals are achieved by installing short-term, low-cost pilot projects (such as temporary bollards) to test out long-term, permanent improvements to the built environment. Some of the recommended routes in this plan could initially be jumpstarted with this program to advertise potential new routes to the community, allowing residents and visitors to test new routes before major improvements are permanently installed. Additionally, Tactical Urbanism can include the use of vehicle parking spaces for other uses, such as outdoor patio dining. Creating a Parklets Program in line with Tactical Urbanism methods would help local businesses gain publicity, minutely expand their service areas, and create a more dynamic and active street that is more enjoyable to traverse.

## **Recommendation 2. Perform Outreach through Community Events and Gamification**

Encouraging residents and visitors to view the city from a perspective outside of their cars will take a bit of effort as Hudson is a suburb community where the majority of households have at least 2 cars. This encouragement can be done through outreach initiatives stemming from community events and games. Examples of such events and games could include:

- Open Streets
- 5k/10k/Marathon races
- Walk with the Mayor
- Bike to Work Week
- Bike Month events during May
- Geocache Hunting
- Historic Hudson Walking/Biking Self-Guided Tour

Spreading the word of these events and games could be done via social media, the local newspaper, and direct mailings to properties. The City could also include these events in its quarterly newsletters. Local media have a high level of interest in stories related to public welfare, community successes and pedestrian safety. The media can be alerted to pedestrian-related efforts through press releases and

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invitations to staged publicity-related events. Positive stories such as ribbon cuttings or community walking events can encourage residents to participate as well as increase awareness and support for on-going efforts.

### **Recommendation 3. Implement a Wayfinding Program**

A bicycle and pedestrian wayfinding system would consist of comprehensive signing to guide bicyclists to their destinations along preferred bicycle routes. Typically, signage is placed at “decision points” or intersections where two or more trails meet. According to NACTO, there are three types of signs:

- **Confirmation Signs** (indicate that users are on a designated trail and makes motorists aware of the route)
- **Turn Signs** (indicates where trails turn from one street to another)
- **Decision Signs** (marks the junction of two or more trails and informs users of designated routes to access key destinations)

Install signage throughout city dictating directions to main attractions such as parks, hospitals, commercial centers, civic or other community destinations, bridges, the library, etc. Signage could include distance and walk time notes to notify people it would take X amount of time to walk to attractions from current locations they are.

Benefits of a wayfinding program are that it helps familiarize users with the local trail network, identifies the best routes to destinations, visually indicates motorists are driving along a designated route and makes them use caution, and it helps to overcome a “barrier for entry” for infrequent bicyclists and pedestrians. Additionally, signage that includes mileage and travel times to destinations may help to minimize the tendency to overestimate the amount of time it takes to travel by walking or biking, thereby incentivizing more frequent use of the trail system.

Drafted examples of signage for the City of Hudson are included in Appendix D-1.

### **Recommendation 4. Education and Awareness Programming**

Like Recommendation 2, public outreach should also incorporate education and awareness of bicycle and pedestrian safety. This can be done through media and public service announcements, safety training and education of police officers, providing defensive driving/biking/walking courses through community programs, and partaking in and educational “Bike & Walk to School Day” using the Bike and Walk Bus technique (kids bike or walk to school together in a group). Further educational materials could be shared to the community via direct mailings and the community newsletter.

### **Recommendation 5. Attain “Bicycle-friendly” Community Status**

The Bicycle Friendly Community (BFC) campaign is an awards program that recognizes municipalities that actively support bicycling. The League of American Bicyclists (LAB) administers the Bicycle Friendly Community program. A Bicycle-Friendly Community provides safe accommodation for cycling and encourages its residents to bike for transportation and recreation. Communities that are bicycle

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friendly are seen as places with a high quality of life. This often translates into increased property values, business growth and increased tourism. Bicycle-friendly communities are places where people feel safe and comfortable riding their bikes for fun, fitness, and transportation. With more people bicycling, communities experience reduced traffic demands, improved air quality and greater physical fitness.

The core of the BFC program is a balanced approach to Engineering, Education, Encouragement, Enforcement and Evaluation, the Five Es. Each of these categories is scored in the application through a series of detailed questions. A community must demonstrate success in each of these areas in order to be considered eligible for an award. Communities with significant achievements in these areas receive awards which are given at Bronze, Silver, Gold and Platinum levels. There is also an honorable mention category for communities that do not qualify for a higher-level award but have demonstrated progress towards future success.

### **Recommendation 6. Attain “Walk-friendly” Community Status**

Walk Friendly Communities (WFC) is a national recognition program developed to encourage towns and cities across the U.S. to establish or recommit to a high priority for supporting safer walking environments. The WFC program recognizes communities that have shown a commitment to improving walkability and pedestrian safety, mobility, access and comfort through comprehensive programs, plans and policies. Communities can apply to the program to receive recognition in the form of a Bronze, Silver, Gold, or Platinum designation. There is no cost to apply for a WFC designation although it is estimated to take approximately 20–60 hours of time to complete an application. Further information is available at [www.walkfriendly.org](http://www.walkfriendly.org). Questions about the program can be directed to [info@walkfriendly.org](mailto:info@walkfriendly.org). The WFC program is maintained by the University of North Carolina Highway Safety Research Center’s Pedestrian and Bicycling Information Center, with support from a number of national partners.

## **Policy Recommendations**

As the previous section outlines programmatic improvements, this section highlights policies that could be implemented to assist with facility and program execution.

### **Recommendation 1. Establish a Complete Streets policy.**

As noted in St Croix County’s Bicycle and Pedestrian Plan, “a well-crafted Complete Streets policy is an effective way to ensure the implementation of bicycle and pedestrian improvements as part of street and road projects, as well as a tool to increase safety and quality of life.” The adoption of such a complete streets policy would facilitate context-sensitive design when city roads are constructed or reconstructed. The County, in its encouragement of each municipality adopting a Complete Streets

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policy, has developed a model policy (based on the National Complete Streets Coalition’s “Best Complete Streets Policies of 2015” report) to adopt. The County also provides guidance for modifying the language as needed for municipalities. This model policy is provided in Appendix E-1.

**Recommendation 2. Update municipal code to require bicycle and pedestrian facilities within new and infill developments.**

Regulatory initiatives can be a powerful tool to incorporate bicycle accommodation within new developments. Zoning Code, Subdivision Code, and Downtown Street Design Standards could require new developments in various land use types to provide bicycle parking (either long-term and/or short-term parking types) and end-of-trip facilities (showers, water bottle refill stations, bike fix-it stations, etc) based on the number of employees and floor area of various land uses. In line with this, flexible parking standards with reduced motor vehicle parking requirements for employment sites could be outlined as a means of incentivizing such improvements.

Meanwhile, for existing businesses and multi-unit dwellings, private sector partnerships should be in place to encourage these entities to provide bicycle parking and end-of-trip facilities. This can include:

- Working with business organizations to replace on-street parking in strategic locations with seasonal, higher capacity bicycle parking opportunities (such as bicycle corrals) that provide good access to local businesses.
- Developing a program to support businesses in existing developments to retrofit existing buildings to provide bicycle parking.
- Working with the private sector to provide bicycle repair and/or retail and rental services.
- Drafting a sunset policy that businesses must provide bicycle and pedestrian facilities within one or two years.

**Recommendation 3. Continuation of a Dedicated Bike Ped Funding policy.**

The City of Hudson drafts a Capital Improvement Plan every two (2) years, with improvements categorically separated based on types of facilities. This CIP includes categories for Trails and Sidewalks. Each year, the City has set aside \$50,000 for trails and \$75,000 for sidewalks. Although this recommendation is the continuation of these funding amounts, city staff may review the potential of expanding these funds for bicycle and pedestrian facilities in the future.

**Recommendation 4. Establish a Maintenance Policy for Snow Removal, Signal Timing and Equipment Updates.**

Currently, a snow removal ordinance exists and places the onus on property owners to keep the sidewalks in front of their homes clean, safe, and open. However, as some property owners in Hudson are "snowbirds", traveling south during winter months, those properties and their sidewalks sit idly collecting more and more snow. And when neighbors walk over these snow patches, they get

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packed down and harden into uneven ice layers, creating unsafe conditions for pedestrians. Therefore, city staff will review the existing policy and funding to determine if it is possible to hire additional staff to remove snow from sidewalks. Meanwhile, there are several trails throughout the city that are typically maintained by homeowner's associations. City staff has shown interest in also reviewing the possibility of removing snow from multi-use trails during winter months as well.

Updates to pedestrian crossing signal timing and equipment occur on an as-needed basis, usually when construction projects happen. However, it is recommended that a citywide review occur for all signal timings and equipment every 3-5 years as traffic levels fluctuate with increases in development and as equipment continues to age.

**Recommendation 5. Establish a Continuous Canopy policy.**

Trees and tree canopy not only make walks and bike rides more enjoyable and pleasing, protecting users from harsh sun rays and heat. They also play an economic role by increasing housing values and alleviate costs related to erosion. Therefore, it is in the City's interest to plant trees within boulevards where possible, especially along trails and sidewalks. Prior to establishing a Continuous Canopy policy, the city should undergo an Urban Tree Canopy Assessment as noted by the US Department of Agriculture's Forest Service. Such assessments are beneficial toward a municipality's broader urban greening and sustainability initiatives as information regarding the extent of impervious surfaces, socioeconomic and health data, traffic density, and heat island maps are reviewed prior to making policy decisions.

More information on Urban Tree Canopy Assessments can be found at the link below:

[https://www.fs.usda.gov/sites/default/files/fs\\_media/fs\\_document/Urban%20Tree%20Canopy%20paper.pdf](https://www.fs.usda.gov/sites/default/files/fs_media/fs_document/Urban%20Tree%20Canopy%20paper.pdf)

**Recommendation 6. Create an ADA Transition Policy for a more walkable and rollable community.**

When communities accommodate for users of all ages and abilities, they become accommodating for everyone at every stage of life and circumstance. One never knows when or if a disability will impact their life, so planning ahead is vitally important for the community as a whole. The creation of an ADA Transition Policy will bring to the forefront key issues and problems impacting those in the community with disabilities, such as upgrading curb ramps and road crossings, providing structural changes to buildings to provide program access, offer programs in an alternative and accessible manner, modify policies and ordinances to ensure nondiscrimination, and provide auxiliary aids as a means of effective communication.

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### **Recommendation 7. Establish a Safe Streets and Roads for All (SS4A) Action Plan.**

A grant opportunity managed through the US Department of Transportation, the Safe Streets and Roads for All Action Plan initiative supports regional, local, and Tribal actions toward zero deaths and serious injuries on the nation's roadways. An Action Plan must consist of the following:

- Leadership commitment and goal setting (via ordinance, resolution, or policy) by a high-ranking official and/or governing body such as the Mayor or Common Council
- A planning structure (via committee, task force, implementation group or similar body)
- Safety analysis reviewing existing conditions and historical trends within the City as well as analyzing systemic and specific safety needs
- Engagement and Collaboration with the public and all relevant stakeholders
- Equity considerations for underserved communities via an equity impact assessment
- Assessment of existing policies and processing and proposing changes as necessary
- Identifying projects and strategies that address safety problems described in the Action Plan
- Measurement of progress over time after the Action Plan has been developed

More information on Action Plans and how to apply for grant funding can be found at the links below:

<https://www.transportation.gov/grants/SS4A> &

[https://safety.fhwa.dot.gov/ped\\_bike/ped\\_focus/docs/fhwasa17050.pdf](https://safety.fhwa.dot.gov/ped_bike/ped_focus/docs/fhwasa17050.pdf)

### **Recommendation 8. Creation of a Design Guidelines Toolkit for bicycle and pedestrian infrastructure.**

Design Guidelines are important for establishing a specific standard that all bicycle and pedestrian facilities should be held to. There are many recommended resources that provide guidance on how to design alternative transportation infrastructure. These entities include:

- The American Association of State Highway and Transportation Officials (AASHTO)
- The National Association of City Transportation Officials (NACTO)
- The Federal highway Administration (FHWA)
- The Wisconsin Department of Transportation (WisDOT)

Most often these design guidelines will be helpful during a project's pre-development phase. For example, the creation of Design Guidelines could illustrate bicycle parking and end-of-trip facility designs that can be provided to developers and building managers to further support implementation of high-quality connections and network facilities. The creation of this toolkit will also bring current alternative transportation technologies to the forefront of planning and design discussions. Such technologies could include:

- Lead Pedestrian Interval (LPI) lights that allow pedestrians to enter the road before cars are given the green light. This in turn gives pedestrians time to claim the road before cars blindly turn, thereby reducing turning crashes & reducing severity of turning crashes.
- Automatic pedestrian "recall" pushbuttons which automatically display pedestrian walk signals on every signal cycle during certain times of the day, such as during school crossing times.

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# Implementation

This citywide Bicycle and Pedestrian Master Plan recommends projects and programs that will improve the physical pedestrian and bicycle environment and simultaneously help the City of Hudson reach its public health and sustainability goals; however, implementation of the projects and programs will take some time and funding to implement. This section lays out the strategy for implementing projects and programs, as well interdepartmental cooperation, and year-round maintenance of both existing and proposed facilities.

## Funding Sources

This section describes various sources of funding available to plan and construct bicycle and pedestrian facilities, or to provide awareness, encouragement, or education programs. Pedestrian projects and programs are funded through multiple sources, and not all sources apply to all projects. Many sources require a local funding match, and most are competitive based on project merit and adherence to grant criteria. This section covers federal, state, regional, and local sources of pedestrian funding, as well as some non-traditional funding sources that have been used by local agencies to fund pedestrian infrastructure and programs. Therefore, the funding sources are divided into three overarching category types.

### TYPE 1. Grant Programs and Charitable Contributions

This type of funding includes Transportation Alternative grants, Recreational Trail Aids Program grants as well as contributions from private and non-profit foundations, or donations from individuals.

**Surface Transportation Program - Urban (STP-U)**: funds in small urban areas (5,000-200,000) may be used by localities for bicycle transportation facility and pedestrian walkway projects on any Federal-aid highway. Cannot be undertaken on a road functionally classified as a local road or rural minor collector. Funds may be used for maps, brochures, and public service announcements related to safe bicycle use and walking. Can cover up to 80% of project costs. Administered by WisDOT and selects projects via a competitive process. **Deadline to apply for Surface Transportation Program – Urban (STP-U) funding is mid-October every odd-numbered year (e.g. applications will next be due in October 2023).**

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**STBG Transportation Alternatives Set-Aside (STBG-TA):** projects exceeding \$300,000 are the best fit for this program since significant amount of admin work is involved. Eligible activities include planning, design, and construction of bicycle and pedestrian facilities, recreational trails, safe routes to school projects, community improvements such as historic preservation, and environmental mitigation related to stormwater and habitat connectivity. WisDOT administers this program. Meanwhile, the Recreational Trails Program portion is administered by the DNR. Cost share for STBG-TA projects is 80% federal with 20% local share. However, if a project exceeds \$600,000 in total costs, the share split will become 60% federal and 40% local. Projects are scored based on criteria such as “importance to regional transportation system and supports regional development framework” (35 pt), “access and service area” (20 pt), “congestion mitigation” (5 pt), “safety enhancement” (15 pt), “environment” (5 pt), “equity” (15 pt), and “cost efficiencies” (5 pt). **Deadline to apply for Transportation Alternatives Set-Aside (STBG-TA) funding is around the end of January every even-numbered year (e.g. applications will next be due in January 2024).**

**Recreational Trails Program (RTP):** eligible activities (in order of priority) are maintenance or restoration of existing trails, development or rehabilitation of trailside/trailhead facilities and trail linkages, construction of new trails, and property acquisition for trails. This is the only federal transportation funding source that can be used for maintenance activities. Funds are administered by the DNR and have a cap of \$45,000 per grant per fiscal year. **Deadline to apply for the Recreational Trails Program (RTP) is typically May 1 of each year.**

**Highway Safety Improvement Program (HSIP):** The FAST Act continues HSIP to achieve a significant reduction in traffic fatalities and serious injuries on all public roads, including non-State-owned public roads and roads on tribal lands. The HSIP requires a data-driven, strategic approach to improving highway safety on all public roads that focuses on performance. Seven percent of each State’s annual STBG Program funds is set aside for Highway Safety Improvement Program and Railway-Highway Crossing Program (intended to address bike/ped safety at hazardous locations among other issues). The federal funding ratio for HSIP funds is usually 90%, requiring a 10% match of state and/or local funds. **Deadline to apply for the Highway Safety Improvement Program (HSIP) is mid-February every 3 years. The next application deadline will be February 2025.**

**Highway Safety Grant Program (Section 402):** Referred to as Section 402 funds is administered by WisDOT. Federal 402 funds are used for pedestrian and bicycle public information and education programs. Funds are distributed to states annually from the National Highway Traffic Safety Administration (NHTSA) according to a formula based on population and road mileage. Government agencies or government-sponsored entities are eligible to apply for 402 funds. Typical funding ratio for HSIP funds is 90%, requiring a 10% match of state and/or local funds. **Deadline to apply for HSIP funding is in February of each year.**

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**Rebuilding American Infrastructure with Sustainability and Equity (RAISE):** Originally created under the American Recovery and Reinvestment Act (ARRA) in 2009 as TIGER grants, the RAISE program helps communities large and small fix and modernize their infrastructure. Projects are rigorously reviewed and selected based on merit. Projects are evaluated on statutory criteria of safety, environmental sustainability, quality of life, economic competitiveness and opportunity, state of good repair, partnership and innovation. New this year, under the Bipartisan Infrastructure Law, 2022 RAISE applications will also be evaluated on the criteria of mobility and community connectivity. The Department will assess projects for universal design and accessibility for travelers, as well as consider how proposals increase mobility for freight and supply chain efficiency. Recently funded projects under this program include the development of bus rapid transit lines, highway and bridge repair, “rails-to-trails” projects, and dock repairs. The United States Department of Transportation manages these funds and awards are a minimum \$5 million in urban areas (>50,000), and \$1 million in rural (<50,000). Federal funding covers no more than 80% share, meaning local share must cover at least 20% of funded projects. **Deadline to apply for RAISE funding is mid-April each year.**

**Associated Transit Improvements (ATI):** Federal Transit Administration (FTA) supports bike access improvements through this program. This program includes, but is not limited to, Enhanced Mobility of Seniors and Individuals with Disabilities (S. 5310) which could fund bike improvements that provide access to an eligible public transit facility, funded at 80% federal share; and Formula Grants for Rural Areas (S. 5311) which includes within its eligible projects capital and planning for bike routes to transit, bike racks, shelters, and equipment for public transportation vehicles. Investments in bike access to public transportation (such as electric scooter or bike share stations) can help the City of Hudson promote the use of transit and provide better access to the public. These funds are managed by WisDOT. **Deadline to apply for the Associated Transit Improvements (ATI) grants is December of each year.**

**Federal Lands Transportation Program (FLTP) and Federal Land Access Program (FLAP):** Office of Federal Lands Highway (FLH) manages several programs that can be used for a wide range of transportation projects for planning and construction, including the development of bike/ped facilities adjacent to or on federal lands. The FLTP is a performance management-based program with annual performance measured against baseline conditions and set goals. Partners include the National Park Service, Fish and Wildlife Service, USDA Forest Service, Bureau of Land Management, and US Army Corps of Engineers. The FLAP emphasizes access to and through Federal Lands for visitors, recreationalists, and resource users, with an emphasis on high-use recreation sites and economic generators. The Federal Lands Planning Program is funded through a maximum set-aside of 5% from FLTP and FLAP to carry out the long-range system-wide transportation planning and coordination, asset management, data collection activities for Federal Lands, including tribal transportation facilities and other federally owned roads open to

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public travel. Wisconsin falls under the Eastern Federal Lands jurisdiction. **Deadline to apply can be found at the following website:** <https://highways.dot.gov/federal-lands/programs-access>

**Carbon Reduction Program (CRP):** The Bipartisan Infrastructure Law of 2021 (BIL) establishes the Carbon Reduction Program, a new federal program which provides funding for projects that reduce transportation emissions and requires states to develop comprehensive carbon reduction strategies. Most relevant to this master plan, funding can be put towards the construction, planning, and design of on-road and off-road trail facilities for pedestrians, bicyclists, and other nonmotorized forms of transportation. Federal share is maximum 80% with 20% non-federal match. **Deadline to apply is annually in May.**

**Safe Streets and Roads for All (SS4A):** In addition to the previous CRP grant, the Bipartisan Infrastructure Law of 2021 also established the new Safe Streets and Roads for All (SS4A) discretionary program with \$5 billion in appropriated funds over the next 5 years. This program funds regional, local and Tribal initiatives through grants to prevent roadway deaths and serious injuries. Eligible activities to receive funding include: the development or updating of comprehensive safety Action Plans; conduction of planning, designing, and development activities in support of an Action Plan; and carrying out projects and strategies identified in an Action Plan. **Deadline to apply is annually in mid-September.**

Other funding options are also available through the U.S. Department of Transportation Transit, Highway, and Safety specifically for projects pertaining to bicycle and pedestrian opportunities. Below is a link to the Department’s website outlining activity/project types and which programs offer funding for those project types.

[https://www.fhwa.dot.gov/environment/bicycle\\_pedestrian/funding/funding\\_opportunities.pdf](https://www.fhwa.dot.gov/environment/bicycle_pedestrian/funding/funding_opportunities.pdf)

The following table details each of the listed funding options in this plan, describing what types of improvements to the City of Hudson’s infrastructure (network, policy, or program) are permitted under each grant program.

Activity	FTA/ATI	HSIP	NHPP/NHS	STBG	STBG-TA	RTP	PLAN	402	FLH	SS4A
Access improvements to public transportation	x			x	x				x	
Bicycle and/or Pedestrian Plans	x			x			x		x	x
Bicycle lanes on road	x	x	x	x	x				x	
Bicycle Parking	x			x	x				x	
Bike racks on transit	x			x	x				x	
Bicycle share (capital/equipment; not operations)	x		x	x	x				x	
Bicycle storage or service centers	x			x	x					
Bridges / overcrossings	x	x	x	x	x	x			x	
Bus shelters	x			x	x				x	
Coordinator positions (State or local)				x	x					
Crosswalks (new or retrofit)	x	x	x	x	x	x			x	x
Curb cuts and ramps	x	x	x	x	x	x			x	
Helmet promotion				x	x			x		x
Historic preservation (bike, ped, transit facilities)	x			x	x				x	
Land/streetscaping (bike/ped route; transit access)	x			x	x				x	
Maps (for bicyclists and/or pedestrians)	x			x	x			x		
Paved shoulders		x	x	x	x				x	
Police patrols				x	x			x		
Recreational trails				x	x	x			x	
Safety brochures, books				x	x			x		x
Safety education positions				x	x			x		x
Shared use paths / transportation trails	x	x	x	x	x	x			x	
Sidewalks (new or retrofit)	x	x	x	x	x	x			x	
Signs / signals / signal improvements	x	x	x	x	x				x	x
Signed bicycle or pedestrian routes	x		x	x	x				x	
Spot improvement programs	x	x		x	x	x				x
Traffic calming	x	x	x	x	x					x
Trail bridges		x	x	x	x	x			x	
Trail/highway intersections		x	x	x	x	x			x	x
Training				x	x	x		x		
Tunnels / undercrossings	x	x	x	x	x	x			x	

**FTA/ATI:** Federal Transit Administration Capital Funds and Associated Transit Improvement

**HSIP:** Highway Safety Improvement Program

**NHPP/NHS:** National Highway Performance Program (National Highway System)

**STBG:** Surface Transportation Block Grant Program

**STBG-TA:** STBG Transportation Alternatives Set-Aside

**RTP:** Recreational Trails Program

**PLAN:** Statewide or Metropolitan Planning

**402:** State and Community Traffic Safety Program

**FLH:** Federal Lands Highway Program (Federal Lands Access Program, Federal lands Transportation Program)

**SS4A:** Safe Streets and Roads for All Grant Program

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Aside from grant funding, there is also the option of obtaining charitable donations. Charitable donations could come from two sources: local funding, and non-governmental funding.

For local funding options, standalone projects are most often funded through a municipality's General Fund (if grants do not cover the bill). Otherwise for projects with a longer life than street markings, General Obligation Debt can be used in the same manner that many street or other infrastructure projects are financed.

Non-governmental funding options include philanthropic foundations, or direct contributions. **Private foundations and non-profit charitable foundations** are potential sources of funding for bikeway and trail projects. In addition to seeking grants from a foundation's existing assets, businesses and organizations could be encouraged to "adopt" or sponsor segments of a trail or on-road bikeway to help fund ongoing maintenance. **Direct Contributions** would include developing a giving program that allows individuals to make direct contributions on utility bills or property tax bills. Could also involve a contribution option on the City's website. Additionally, partnerships could be forged with one or more non-profit foundations to develop campaign materials and create a dedicated fundraising website.

## **TYPE 2. User Fees**

**Reallocate revenues from existing user fees (e.g., parking meter revenues) or create new user fees (e.g., annual or daily trail passes or bicycle registration fees).**

## **TYPE 3. Property Taxes**

**Utilize increased levy resulting from new construction and increased valuations or increase the mill rate slightly.**

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## Maintenance

Maintenance of bicycle and pedestrian network facilities will encourage continual use of facilities by residents and visitors. Maintenance activities include crack filling, fog sealing, patching, repaving, updating pedestrian ramps to meet ADA standards, and clearing facilities of trash and debris. These actions are all critical to maintaining the comfort and attractiveness of these facilities.

The City of Hudson has identified the following ways to improve maintenance:

- Proactively seek funding and employment sources to dedicate for maintenance of bicycle and pedestrian facilities.
- Regularly identify maintenance needs and execute bicycle and pedestrian network facility maintenance.
- Coordinate maintenance needs with roadway and trail owners.
- Seek opportunities to level sloped sidewalks at driveways in residential neighborhoods
- Synchronize improvements recommended in this Plan with implementation strategies in other Citywide planning and development efforts.
- Increase Public Works staffing capacity and the equipment fleet to proactively lead maintenance efforts, including the possibility of taking on snow removal responsibilities of the entire trail network.

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# Departmental Tasks of Implementation

Each department shall have their own designated implementation duties as outlined in this plan.

**Community Development** shall be involved in development site plan reviews, policy drafting, and long-range planning.

**Public Works and Parks** shall perform development site plan reviews, policy drafting, physical maintenance and evaluation of the trail and sidewalk system via specific performance measures. These performance measures should be benchmarked against similar-sized communities when possible. Upon the approval of this plan, performance measures should be immediately tallied and recorded. Examples of performance measures include:

- Total miles/ft of sidewalks/bike lanes/multi-use trails
- Total number of marked crosswalks
- Total number of unmarked crosswalks
- Number of crosswalks repainted (annual)
- Total miles/ft of sidewalk reconstructed
- Total miles/ft of sidewalks/trails/bike lanes constructed
- Number of sidewalks not properly cleared on time during winter
- Citations issued to residents for not clearing sidewalks
- Bike lanes/sidewalks/trails as % of total roads
- All bike/ped infrastructure combined as a % of total roads
- Number of controlled intersections
- Number of uncontrolled intersections
- Designated safe route to school paths (in miles)
- Number of bike/pedestrian accidents
- Number of bike racks (public and private-owned)
- Number of businesses that have bike parking options
- Number of children participating in walk to school day(s)
- Number of ped/bike focused marketing campaigns
- Police department hours spent promoting bike/ped safety
- Number of special events featuring bike/ped safety or training

Lastly, the **Police Department** shall register bicycles and promote bicycle and pedestrian safety campaigns throughout the community.

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## Images:

*Best* by The Icon Z from NounProject.com

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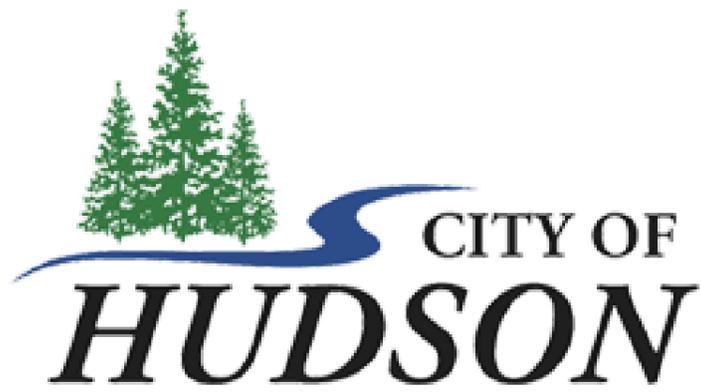
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# **APPENDIX A-1**

## **POLCO Survey**

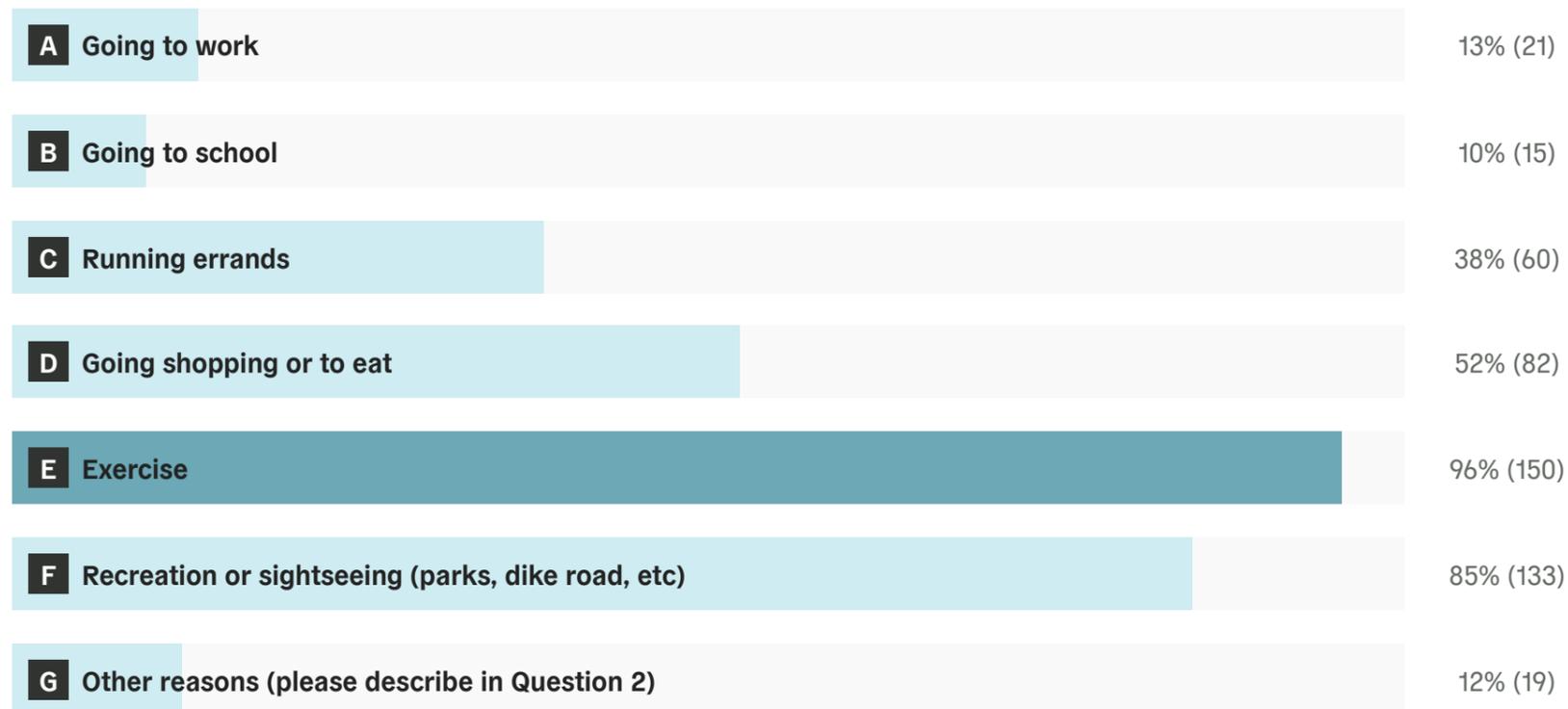


# Hudson Bicycle & Pedestrian Survey

Survey Results  
FINAL

11/13/2020

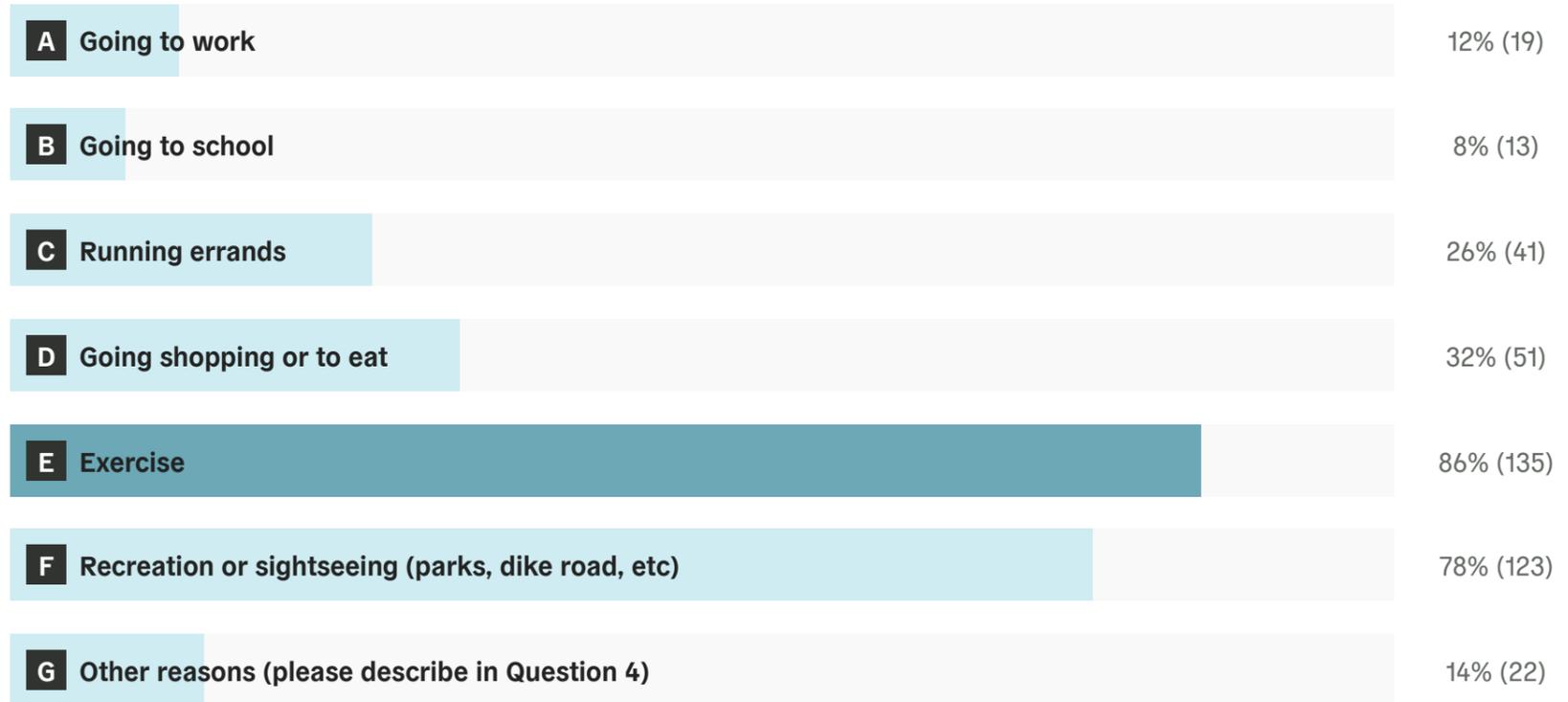
**Which of the following reasons causes you to walk outside? (Please check all that apply)**



**If you marked "Other" in Question 1, please explain here:**

- Walking my dogs
- Going to the pickleball courts
- NA
- Mass, church functions, obligations, & celebrations such as wedding, funerals, baptisms, confirmations, Eucharistic Adoration, Stations of the Cross, Holy week, visits with family, friends, neighbors, voluntary work, community events, emergencies, voting, fresh air, sunshine, gardening, yard work, outside home projects, weather alarms, accidents, & dog walks.
- Walking in my neighborhood
- Walk our dog
- Walking the dog
- walking the dog
- Walking the dog
- I routinely walk to the YMCA
- Visiting bars and not driving.
- Visit neighbors
- Going to church.
- Visit neighbors
- Walk dog
- Visiting friends or family.
- mental health, clearing my head.
- If I'm at an establishment downtown and have a couple of drinks, I leave my car and walk home. It's nice to be able to do that, since we have so few public transportation options in Hudson.
- Walk our dog.
- Walk dog

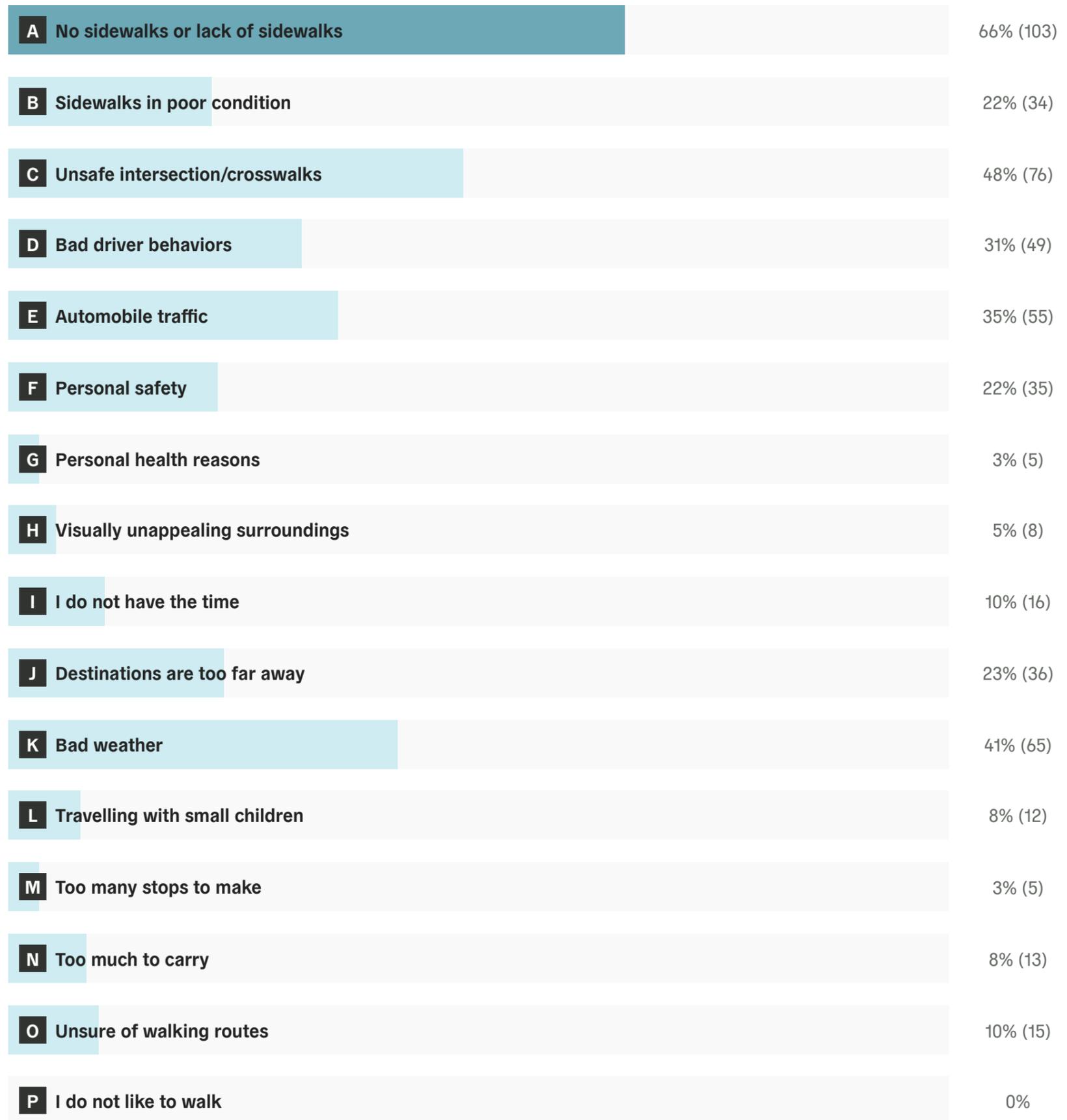
**Which of the following reasons causes you to ride a bicycle? (Please check all that apply)**



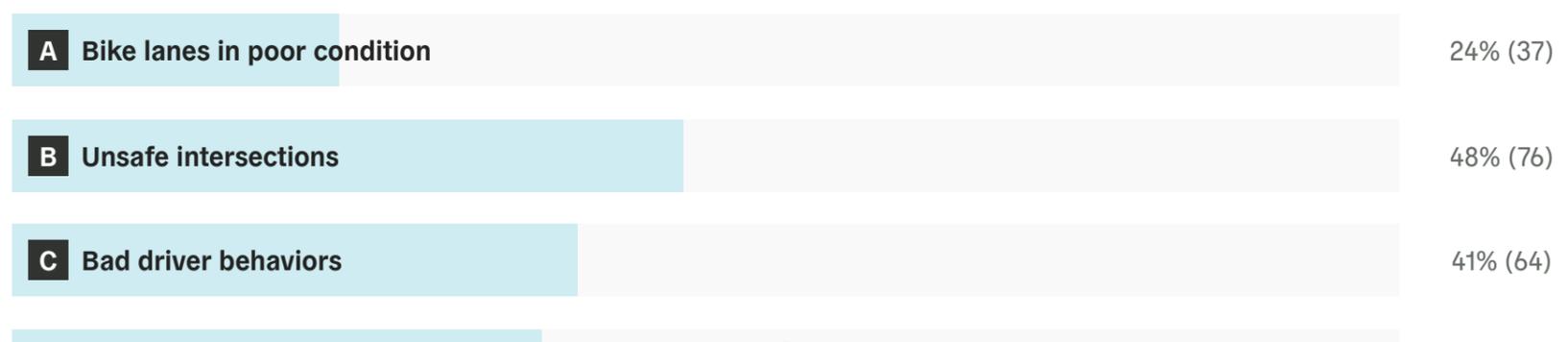
**If you marked "Other" in Question 3, please explain here:**

- None.
- Avoiding use of the car
- Don't ride a bike
- NA
- Don't ride
- Don't own a bike.
- I do not like to ride a bicycle and don't own one
- I don't ride a bicycle at this time.
- Nothing would cause me to ride a bike on a bike trail.
- mountain biking
- I don't ride a bicycle
- I walk, I do not bike
- We don't ride bikes. Getting too old.
- Do not bike.
- I don't ride a bicycle
- I don't ride a bicycle.
- don't ride bike
- Visiting friends or family
- I used to bike to work every day in Minneapolis when there was a safe route. I WOULD bike to work and to run errands if there was a safe way to do it. I would LOVE it! I did it in winter, fall, spring and summer.
- Fun, family and down hill.
- Can't drive a car due to health issues
- I don't
- I don't ride a bicycle

**Which of the following reasons causes you to NOT WALK more frequently? (Please check all that apply)**



**Which of the following reasons causes you to NOT BIKE more frequently? (Please check all that apply)**



<b>D</b> Automobile traffic	38% (60)
<b>E</b> Personal safety concerns	24% (38)
<b>F</b> Visually unappealing surroundings	3% (5)
<b>G</b> I do not have time	5% (8)
<b>H</b> Destinations are too far away	11% (18)
<b>I</b> Bad weather	30% (47)
<b>J</b> Travel with small children	8% (12)
<b>K</b> Too many stops to make	4% (7)
<b>L</b> Too much to carry	8% (12)
<b>M</b> Unsure of bike routes	25% (39)
<b>N</b> I do not have a bike	13% (20)
<b>O</b> No bicycle parking	30% (47)
<b>P</b> No designated bike lanes	63% (99)

How important do you think the following improvements would be in supporting walking and bicycling in Hudson? Please allocate 100 points across all answer choices (i.e. give more points to what you think is more important).

<b>A</b> More sidewalks/bike lanes/signed bike routes/greenways	35% (54.78)
<b>B</b> Maintenance of sidewalks, bike lanes, bike routes/greenways	11% (17.66)
<b>C</b> Improved connections between sidewalks and bikeways	10% (15.6)
<b>D</b> Better intersections (pedestrian signals/crosswalks)	9% (14.17)
<b>E</b> Better street lighting (for evening bike rides or walks)	6% (9.3)
<b>F</b> More separation from vehicle traffic	13% (19.7)
<b>G</b> Education/enforcement for motorists, pedestrians, and bicyclists	7% (10.41)
<b>H</b> More bicycle parking	4% (7.01)

**I** Bicycle route map 5% (8.37)

**Which of the following best describes you? (Please check all that apply)**

**A** Employed full-time 54% (85)

**B** Employed part-time 17% (27)

**C** Full-time student 4% (6)

**D** Part-time student 1% (1)

**E** Unemployed or retired 27% (43)

**Including yourself, do any members of your household attend school? If so, what school?**

**A** Elementary school 17% (26)

**B** Middle school 6% (10)

**C** High School 11% (18)

**D** College 10% (15)

**E** No students in household 56% (88)

**If you have family that goes to school, how do they get to school?**

**A** Walk 8% (12)

**B** Bike 3% (5)

**C** Ride school bus 11% (18)

**D** Drive themselves to school 10% (16)



<b>E</b> I drive them to school	10% (15)
<b>F</b> No students in household	58% (91)

### Please share any additional comments or feedback you have regarding Bicycle and Pedestrian infrastructure in the City of Hudson:

I rode my bike a lot when I lived in River Falls. I rarely ride since I moved to Hudson. This is largely because of geography -- too many hills, not enough through streets so that all the traffic is pushed onto a few high-volume routes which are not safe or comfortable to ride on. Plus the way subdivisions are created these days to be all curving roads that don't connect to anything and lots of cul de sacs, a lot of Hudson is not designed for any mode of transportation except cars.

Glad to see an effort to improve bicycle facilities, Finally!!

There needs to be crosswalks painted at all intersections with sidewalks and consistency in marking the crosswalks. Also there are still numerous intersections with no traffic control which makes crossing even more difficult.

Cross walks on 2nd street from Vine to North Hudson are extremely dangerous and need upgrading before there is a serious accident. I cross 2nd street regularly and have come close to getting struck numerous times.

Bicycle paths NEED to be separate from walking paths

I would much prefer public transportation (a shuttle bus loop in Hudson) to spending oodles of money on bike lanes. That would provide transportation and independence for elderly and young adults too.

Riding my bike on Carmichael across 94, even using the sidewalk, is taking my life in my hands. Trying to get from 2nd street north into North Hudson is a nightmare. Don't understand why there isn't a bike route to connect with the new trail up Hwy 35 to Stillwater. Missing a great chance to increase recreational spending in Hudson. And some bicycle parking in downtown Hudson would be a nice feature, don't you think?

We walk a lot in town. Many sidewalks are impeded by overgrowth of private landowners untended bushes and shrubs. Would be nice if they'd trim them back. Sidewalks in town are generally in fair or reasonable condition. I am glad for this survey because HUDSON IS MISSING A HUGE OPPORTUNITY UNLESS WE IMPROVE OVERALL CYCLING CONDITIONS - it frees up parking, better for the environment, brings more people to town. Need to find a way to connect to trails to other communities, and the St. Croix Crossing trail system. I know it's a challenge, but even slight improvements downtown would help as well.

Get these improvements out of your head(s)! No walking or bike paths!! Simply keep the streets repaired and plowed!

We should not be funding projects for a small vocal minority. QUESTION #7. I do not think it is important. How many ways do I need to say this?

We need more bike friendly roads/ paths that connect neighborhoods to schools, shopping areas and downtown. I would bike more often if these connections could be made. Creating a more bike friendly system will encourage more biking.

Winter residential lack of shoveling is a big issue in the "old neighborhood". Snow and ice buildup makes for slow treacherous walking, or being forced to walk in the street.

We do not need to extend the first street walking path going north. Prefer to leave the neighborhood the way it is. Do not want to increase bikes in our neighborhood. Leave it natural for all the wildlife.

We would LOVE to see more extensive and better bike paths in and around town that is separated from traffic such as the Gateway and Brown's Trail in Stillwater.

I have kids in elementary, middle, and high school. Hudson needs safe bike paths/lanes!! Our family would love to be able to ride from Heritage Greens to downtown Hudson, and it would be amazing if my high schooler could safely ride his bike to school, to the soccer fields, and to Catalyst for indoor soccer training if weather allows. The only thing preventing him from doing so is the lack of safe bike paths.

I prefer to bicycle on paths/trail ways that are not connected to or directly adjacent to the roadway with vehicles. Often the vehicles come too close to the bicyclist and the path/trail way is often very sandy and/or rocky making it difficult to bicycle on. The same would apply to walking/pedestrian paths/trail ways.

I love using the sidewalks & walking paths in my part of town (N Carmichael area) for walking & riding my bike. I would do it more at night if there were better lighting and I'd go on longer/more frequent walks & rides if sidewalks were extended further down Carmichael toward town, as well as out the other way toward Willow River park.

Sidewalk maintenance is important for those walking but cycles below on protected bicycle lanes/streets. Just a radical idea, but how about sidewalks on both sides of the street. Also streets should be designed or upgraded with traffic calming enhancements

which are well proven through out our area and beyond. Please create implement a cycle routing plan that allows safe, convenient access to our entire community.

Would love to see a connection and Carmichael and Coulee south on Carmichael so kids can safely bike and walk from MS and HS to Culver's and Target area.

I do not want to see paying additional taxes especially for more bike trails. If someone wants to donate & maintain a mile, for awhile, great! People are more likely to drive to a park, get out & walk the area. Bike trails are nice but expensive to build & maintain. And, there's always add-on's. Today, cyclists can be a serious danger to motorists & themselves. They don't obey the road signs. Take a survey of middle or High School kids & ask; how many are riding their bikes to school? Today's culture is not about economics but about a kind of leisure with convenience at the taxpayers expense. I rode more often in my youth & used my bike as a back up in getting to work. And, not many folks back then did this & no matter what you spend on bike & walk trails, its not going to change the nature of things. People like their cars & they NEED them for everything especially winter months. The American automobile gives everyone convenience, safety, independence & more time!

Winter maintenance of sidewalks and pathways must be improved to reduce slipping and falling hazards and risks.

A bike trail is a TOTAL waste of money! It will be used by very few. No protection from predators, thieves, thugs from the cities. Foolish to pay for a bike trail. Foolish!

Access and safe bike trails and lanes are SO important for an active lifestyle and quality of life. Would be fun to have community bike events, too!

I don't ride around the city often, but I frequently ride on bike trails, and into rural Hudson, I am interested in a bike trail that would make a loop from Stillwater to Hudson with trails on both sides of the St Croix River.. Probably to late to make a bike trail on the old rail bed from Hudson to New Richmond. Some of the rail bed is visible, and can be seen on Google Maps

Reasons for not biking should have included theft - i.e. fear of bike being stolen. There are plenty of sidewalks in town, number of sidewalks is not an issue.

Strongly support improving the bike/pedestrian infrastructure. Hudson does not have a safe route to bike or walk as a family without going on streets where there are no sidewalks or safe bike routes away from traffic. We often leave town to bike in other surrounding towns with safer bike paths and often spend money in those towns buying lunch, coffees, etc. Improving these would be a draw for Hudson and help the local businesses as well as keep its current community members in town more. "Encouraging community."Hudson is a healthy flourishing community and actions in this department need to be taken to continue on this path.

Please connect the Hudson Meadows Neighborhood on Old Highway 35 via sidewalk/bike trails to trails and commercial sites.

Connecting the existing pathways, would, in my opinion, be step one. There's no point in having a path that doesn't really go anywhere. Adding pathways that offer a safe, appealing atmosphere is also the only way to have people utilize the paths- having a sidewalk right next to the overpass with only a metal wire fence as a barrier is not appealing, feels unsafe for many people, and makes the city look like it doesn't care.

When I did have a student in school she either rode the bus, I drove her or eventually she drove herself. To think we're going to add bike or walking paths and suddenly students will use those methods is nuts. We lived across the street from the High School when my daughter was there she still drove and paid the parking fee herself.

We are 20 years behind in city planning compared to all local municipals. We need to grow up and support long-term benefits

I do not support assessing property owners for sidewalks on their property. They are probably the least-likely to use them; they shouldn't have to pay for them. I think those of us that use sidewalks, bike trail, etc should all share the costs.

Thank you for your efforts on improving the bicycle and pedestrian infrastructure in the City of Hudson. I would like to see a stairway or switchback trails to get from Prospect Park to downtown (replace the old rusty stairway that has been removed).

Sidewalks are the most important thing for safety. Sidewalks distance people from traffic and it feels safer to walk in areas with sidewalks. I was raised on Carmichael so I am used to walking literally alongside traffic. This is not the norm. People wouldn't walk home with me after school because the traffic made them uncomfortable. I don't think this is an uncommon sentiment. I'm certainly in the minority of people that will walk or bike regardless because I was raised in those types of conditions and I'm confident in monitoring my own safety. Prodigious more sidewalks first and foremost is probably one of the easiest ways to get people out of the house, especially children and the elderly. While bikers are not supposed to, they can use the sidewalks along with pedestrians because they really aren't used that much by pedestrians in our area (besides downtown and no one tries to bike downtown because it'd suck). Bike lanes should be secondary to sidewalks, but if they can be done, they totally should. I think sidewalks really get people out of their houses and interacting with their neighbors. As someone that would bike a lot from my home on Carmichael, knowing if there was a place to lock up my bike was very important. There were places I couldn't go because there was no where nearby to lock up. Target has a bike rack but Family Fresh did not and you had to lock your bike to a picnic table. Walmart has one but County Market does not (or didn't when I biked a lot). I was more likely to go somewhere like Brugers Bagels or Target because I knew they had racks.

I live in N Hudson but would love to drive less. Everything in on the hill so it is impossible for me to bike to get groceries, household, and pet supplies.

Add some of the flashing speed lights on busy roads. Many drivers are unaware of how fast they are driving. Thanks!

Would really like some sidewalk all alongside CARMICHAEL in order to walk all the way from the intersection of vine all the way down

Thanks for asking the questions of the public!!

Stageline REALLY, REALLY, NEEDS a sidewalk or much wider shoulder. There seems to be lots of people walking or biking this street and it is so narrow it is dangerous.

I think the topography of Hudson makes it difficult to bike anywhere without hauling your bike around. I like biking, but I would rather see more money spent on fixing roads for cars.

Get a sidewalk / bikelane built on Stangeline between the hospital and Hwy 12 ASAP. Similar to Hanley.

More paths please!

Question 3 needs a "Do not use bicycle" option. Our HOA has almost 300 homes and less than 5% ride bikes.

Main thoroughfares like Carmichael, Vine, Wisconsin, 11th Streets should have marked bike lanes on both sides with no parking allowed. The Vine Street reconstruction last year was a great chance to do so, but the city failed.

I feel that our existing infrastructure more than adequately supports pedestrian traffic as well as bicycle traffic

The hill area needs sidewalks paid by the city as all citizens benefit.

Hudson isn't bike friendly. The fact that Vine Street had major work and didn't have a bike lane added shows just how uninspired the planning people in the city are. Also, I like in an area where there are no sidewalks and access roads are in dangerously poor condition. It is not elder-friendly at all. When I get to retire, I will have to move out of Hudson.

Hudson already has pretty good walks and bike paths. This should not be a big budget item. Just keep doing the good job that has been underway for many years.

We need to find ways for people to get around the city that doesn't involve consuming carbon based fuels.

The city needs to add sidewalks to nearly all of the hill area, which it allowed to develop without sidewalks. This should now be at city expense as sidewalks are used by the general public - not just the homeowner!

Vine street is so busy-except for past month. Would be better if 4 way stop was at Diamond Drive because YMCA and businesses there. Would like to see sidewalks on both sides of Vine where new development is on Carmichael and Vine

The sidewalks need to be maintained in winter months. The Hanley / Carmichael intersection has always been hard to get across the last few years. Handicapped can not use these routes at all in winter.

Older neighborhoods need street improvements. It is FREAKING ridiculous that Stone Pine's not that old and has in the past 5 years had upgraded curb and road maintenance while other roads suffer and are quite deteriorated and these are also often in neighborhoods with little to NO sidewalks. I also feel that PEDESTRIANS and BICYCLISTS need to learn to obey traffic laws as well. I have had too many times, especially down town where people don't even look before they cross the road and I've almost hit them because no one was there when I started by. Runners need to stop at stop signs just like vehicles and bikes do. They are not more important and this also then holds up traffic. South end of 2nd St down town needs an additional cross walk, and maybe one of the crossing signs that they can push buttons to help alert drivers, especially at night. It is SO hard to see especially if people are wearing dark clothing. There should also be a pedestrian bridge built by the YMCA...this would allow so many more people to use our city's resources safely, in an already terrible spot to try to even be a vehicle.

More organized events by city and non profits.

With attention to sound fiscal management, I would like more dedicated walking and bike paths. Minneapolis has the best walking/bike path system I've ever seen. The paths are wide for both bikes and walking and I believe Hudson could partner with surrounding cities to expand our paths similarly with a responsible ten year plan make St. Croix county stand out while bringing locations not on roadways into play. I bike many miles per week during lunch in Minneapolis, yet have never ridden in Hudson due to the absence of dedicated paths (I don't trust motorists).

My children love to bike (the above questions wouldn't let me pick more than one answer & I have 1 child in high school, 1 in middle school & 2 in elementary). My oldest especially bikes a lot (hundreds of miles a year) & he is frustrated by the lack of bike paths in Hudson. I'm also frustrated as I worry about their safety biking on roads vs a designated bike path which Hudson needs more of.

We would love more bicycle trails that are separate from car or atv traffic! We'd also love more bike lanes to make it safer and more appealing to bike to downtown businesses and parks. We know many people who drive to Minnesota to use their bike infrastructure because ours is lacking.

I'm a big believer that better bike and ped infrastructure leads to a more vibrant, connected, and healthy community. Thank you for your consideration.

County Rd F would be a great road to expand with a wide, safe bike trail.

We have kids in all three schools. Elementary, HMS, and HHS. All 3 walk and/or ride their bikes to school. Our family would love bike paths and walking paths. They would be well used!

Thank you creating this survey. Hudson has needed to address the poor street conditions for many years now. I grew up in Hudson and teach in Hudson and it is clear for a multitude of reasons Hudson seriously needs to focus on the health of the community as it ties to citizens of all ages being able to travel around town efficiently, safely and separated from automobile traffic. To be able to travel around town under my own physical power (walking/running/biking/rollerblading etc.) without hitting potholes or getting hit by a car would be really great. The St. Croix river valley is beautiful and Hudson would really do well for its businesses and citizens to honor that beauty with beautiful paths for people to safely get around.

There needs to be a better path from lakefront to the amazing 94 river crossing. It is not maintained. And not large enough!

Crossing the interstate is the biggest obstacle for biking in the city of Hudson. Personally I don't Bikes should Be on sidewalks nor do we need bike lanes on every street. Education and share the road needs to be a focus on a small community with low resources. There are plenty of low volume low stop routes in the cities to get places. Educating bikers and peds of those routes is important.

Better education of drivers on what role bicyclists have. I.e. while on the bicycle we are vehicles and not pedestrians, therefore they should not stop at busy intersections to let us by. We will find our time to cross like other vehicles.

Would be VERY interested in more "greenway" type paths into downtown

Moving from Minneapolis to Hudson, this has been one of the biggest disappointments. I rode my bike everywhere in Minneapolis and the kids were with me. The Greenway is safe, apart from traffic, well marked and connects the city to multiple locations for business. If we could make this work in Hudson, it would make quality of life increase!! Plus, more people would be out exercising, just incorporating it in their day which is great for continuity and long-term adherence to exercise and health.

It would be very nice to have bike lanes, especially on the busy downtown streets.

Please add a bike/pedestrian lane on Trout Brook Rd.

Disappointed to see city approve new developments and even annex them (Higher taxes) but provide no shoulder or sidewalk or bike lane to connect homeowners on main road to shopping and retail.

Concern for safety of bikers not following rules of road. (Walking bikes across intersections.)

One of the things I treasure most about Hudson is the great opportunities for walking, and enjoying our green spaces. I believe there could be improved cross-walks (my husband owns Ziggy's, and we practically get run over every time we try to cross the street). MORE IMPORTANT than bike trails: FIX THE HORRIBLE DOWNTOWN PARKING SITUATION!!! Charging people to park until 8 PM instead of 5 PM weekdays, and by adding Saturdays until 8 PM to the mix was a HORRIBLE decision. People visit Hudson because of the quaint downtown and the river - NOT the part of Hudson over the hill that is starting to resemble Woodbury. Yes - I believe opportunities for walking and biking are important for the community. But PARKING and expanding opportunities for public transportation are a MUCH greater priority.

Feel it's critical to connect bike lane from Hudson to the Stillwater/Houlton bike/path network. Safety, support tourism and community with Hudson Area.

Question 10 ... I have one that walks to school (Middle School) and one that drives herself (HS). Hudson GREATLY needs more bike paths. If our family wants to bike, we usually transport our bikes to Stillwater or Lake Elmo in order to find safe paths. It is NOT safe to bike in Hudson, due to the lack of trails and due to crazy inattentive drivers.

When weather is nice and they can walk on lawns student walks but otherwise I need to drive. Not safe behind HHS. Connecting with neighboring community trails would boost tourism revenue in Hudson. Bikers destination.

I walk frequently in the City of Hudson. I would like to see the city better enforce sidewalk maintenance by property owners who neglect this obligation during the winter months.

# **APPENDIX A-2**

## Comprehensive Plan Community Survey

## Community Survey for Comprehensive Plan Update

Please rate the quality of the following in Hudson:

CURRENT RESULTS	176 Total Responses					
	Excellent	Very Good	Good	Fair	Poor	Unsure
Friendly Community	18% (32)	36% (64)	31% (54)	13% (22)	2% (4)	0% (-)
Community Atmosphere	11% (20)	41% (73)	33% (58)	11% (20)	3% (5)	0% (-)
Schools	22% (38)	41% (73)	20% (35)	4% (7)	2% (3)	11% (20)
City Government	3% (5)	16% (28)	38% (66)	26% (46)	7% (13)	10% (18)
Safe Community / Feeling of Security	18% (31)	44% (78)	34% (59)	4% (7)	1% (1)	0% (-)
Activities for Youth	7% (13)	28% (49)	25% (44)	9% (15)	6% (11)	25% (44)
Child Care Facilities	3% (5)	15% (26)	22% (38)	11% (19)	1% (2)	49% (86)
Community Events	12% (21)	36% (64)	35% (62)	14% (24)	0% (-)	3% (5)
Internet Service	7% (12)	26% (46)	34% (59)	17% (30)	14% (24)	3% (5)
Medical Facilities	12% (21)	43% (75)	29% (51)	9% (16)	6% (10)	2% (3)
Housing Options	4% (7)	19% (33)	30% (52)	24% (43)	16% (29)	7% (12)
Opportunities for Quality Jobs	4% (7)	14% (25)	30% (53)	26% (46)	9% (15)	17% (30)
Parks & Open Space	13% (22)	44% (78)	28% (49)	10% (18)	5% (9)	0% (-)
Water Resources Quality	15% (26)	38% (67)	27% (47)	10% (17)	4% (7)	7% (12)
Air Quality	22% (38)	47% (82)	26% (45)	4% (7)	0% (-)	2% (4)
Overall Quality of Life in Hudson	14% (24)	53% (93)	31% (54)	2% (4)	1% (1)	0% (-)

## How important is it for the City of Hudson to promote the following in the next 20 years?

CURRENT RESULTS	176 Total Responses					
	Extremely Important	Very Important	Somewhat Important	Not Very Important	Not At All Important	Unsure
Maintaining Community Atmosphere	35% (61)	48% (85)	15% (27)	2% (3)	0% (-)	0% (-)
Maintaining a Good School System	60% (106)	34% (60)	3% (6)	1% (1)	0% (-)	2% (3)
Maintaining a Safe Community / Security	62% (109)	36% (63)	2% (4)	0% (-)	0% (-)	0% (-)
Promoting Public Health & an Active Community	28% (49)	40% (70)	30% (53)	2% (3)	0% (-)	1% (1)
Promoting Youth Activities	28% (50)	40% (71)	27% (48)	2% (4)	1% (1)	1% (2)
Supporting Existing Businesses	39% (68)	46% (81)	14% (24)	1% (1)	0% (-)	1% (2)
Encouraging New Employment Opportunities	30% (52)	41% (72)	24% (43)	4% (7)	1% (1)	1% (1)
Expanding Shopping Options	9% (16)	20% (35)	36% (63)	25% (44)	10% (18)	0% (-)
Improving Senior Citizen Services	32% (56)	34% (59)	20% (36)	5% (8)	1% (1)	9% (16)
Enforcing Municipal Codes & Development Standards	23% (40)	44% (77)	21% (37)	7% (12)	2% (3)	4% (7)
Maintaining and Improving Local Roads	38% (67)	43% (75)	16% (29)	3% (5)	0% (-)	0% (-)
Encouraging Sustainable Development	36% (63)	31% (55)	22% (39)	7% (13)	3% (6)	0% (-)
Protecting Natural Areas	67% (118)	22% (38)	7% (13)	4% (7)	0% (-)	0% (-)
Protecting Water Resources	73% (129)	23% (40)	3% (5)	1% (1)	1% (1)	0% (-)

## What type of business development should Hudson encourage in the next 20 years?

CURRENT RESULTS				176 Total Responses
	Need More Of	Keep at Existing Level	Need Fewer Of	Unsure
Offices / Business Parks	22% (38)	52% (91)	20% (36)	6% (11)
Retail / Shopping	31% (55)	51% (90)	15% (26)	3% (5)
Neighborhood Services / Businesses	47% (82)	42% (74)	3% (6)	8% (14)
Restaurants	19% (34)	64% (113)	15% (26)	2% (3)
Hotels / Motels	7% (13)	59% (104)	27% (48)	6% (11)
Event / Convention Center	37% (65)	34% (60)	18% (31)	11% (20)
Entertainment / Attractions	45% (79)	40% (71)	11% (19)	4% (7)
Tourism Industry	34% (60)	53% (94)	7% (12)	6% (10)
Industry / Manufacturing	28% (50)	47% (82)	15% (27)	9% (15)

**Are there other types of business development Hudson should encourage in the next 20 years? If so please describe the type and identify where it should be located.**

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 **Anonymous user's Opinion**  
Incorporate clean energy into the overall city plan. There are emerging technologies and jobs that should be encouraged. I would encourage clean energy industry over old tech manufacturing.

 **Anonymous user's Opinion**  
We are rapidly moving into an electric vehicle world. Consequently vehicle charging stations should become a high priority, especially in the downtown retail areas.

 **Anonymous user's Opinion**  
Sustainable energy investments

 **Anonymous user's Opinion**  
I see too much green space disappearing. Instead, we should develop more bike paths and other outside activities to encourage healthy lifestyles.

 **Anonymous user's Opinion**  
Hold the Hudson Farmers' Market at Lakefront Park, instead of in a parking lot along I-94.

 **Anonymous user's Opinion**  
Companies that pay living wages and fit into the values of our community. Companies that encourage employees to increase skill set and training. Professional and manufacturing jobs.

 **Anonymous user's Opinion**  
transportation systems to get people around town as well as into the cities. Food: 1. locally grown and distributed. 2. more community meals.

 **Anonymous user's Opinion**  
Outdoor pool or splash pad

 **Anonymous user's Opinion**  
Outdoor activities that compliment our beautiful National Scenic St Croix River

 **Anonymous user's Opinion**  
Maybe look at some more technical based businesses.

 **Anonymous user's Opinion**  
What happened to the ballfield and shops at the old dog track? Instead there's a huge, ugly factory? We need to build clean energy resources. How about a solar panel field by XCEL?

 **Anonymous user's Opinion**  
Low and medium income housing for those who work in Hudson and St. Croix County.

 **Anonymous user's Opinion**  
I'd love to see a community center with activities for children, an outdoor pool, adaptive playground.

 **Anonymous user's Opinion**  
None, no more.

 **Anonymous user's Opinion**  
Hudson has a good mix, maintain through treating them right or seek similar balance as any leave.

 **Anonymous user's Opinion**  
Unsure

 **Anonymous user's Opinion**  
We do need to develop the corner of Carmichael where there is an empty abandoned barn and home. This has been an eye sore for many many years. A gas station and restaurants would fit nicely there.

 **Anonymous user's Opinion**  
More family friendly places- activity centers, pool, etc

 **Anonymous user's Opinion**  
A grocery store downtown would be very good.



**Anonymous user's Opinion**

I think Hudson should encourage more locally owned businesses instead of franchise or chain businesses. I don't think there's a specific type of business that we need.



**Anonymous user's Opinion**

A small grocery store downtown would be great.



**Anonymous user's Opinion**

Small locally owned-sustainable



**Anonymous user's Opinion**

More tech industry jobs, high skill jobs. We have an abundance of low skill, low pay jobs. I have to work in MN because you have no tech jobs that pay anything close to what I make in MN.



**Anonymous user's Opinion**

A multi-level paid parking garage at 2nd & Vine. Eliminate street parking fees. It works for Stillwater.



**Anonymous user's Opinion**

Mayo Clinic.



**Anonymous user's Opinion**

Adding jobs, yes but increasing population density - NO.



**Anonymous user's Opinion**

Target Supercenter. Community pool/splash pad. Affordable housing while maintaining green space. Better traffic control on Vine and Carmichael as well as the entire Carmichael/94 exchange.



**Anonymous user's Opinion**

Tech isn't mentioned above. Its a top growth area.



**Anonymous user's Opinion**

Kwik trip at Carmichael and vine!



**Anonymous user's Opinion**

Keeping downtown vibrant should be priority. Sprawl up on the hill is poorly planned.



**Anonymous user's Opinion**

Maybe work sharing space and IT.



**Anonymous user's Opinion**

You have a group that has been trying to get an animal shelter in here for 13 years. It is a needed business and I think our area should be supporting their efforts. It is located at exit 10.



**Anonymous user's Opinion**

Baseball team follow through. River Hounds In planned space. VFW location We are the county seat of St. Croix county. We lack a building for our Veterans to coordinate events, have resources available to vets in need. If not now, when? Thank you for your consideration. Ward Avenue? Ballpark/Glenview? The location should have ample parking. Somerset and New Richmond, WI have very active posts that could be guides for best practices.



**Anonymous user's Opinion**

One of the things I hear the most when talking to visitors of Hudson, is the quality of local, independent or very small chain dining. There's always room for more unique options, but not so many duplicates (we don't need 7 sub shops, for example...). Also, light manufacturing and more entertainment options. It would be wonderful to see the new development at the dog track take shape as a destination venue - but so far, we only see industry. The ballpark would be a unique addition to Hudson's community.



**Anonymous user's Opinion**

Hold off on any more TIF for Phillips-Medisize. The Koch Brothers have enough tax freebies and the profits finance issues with which I DISAGREE.



**Anonymous user's Opinion**

more grocery stores



**Anonymous user's Opinion**

I would love a Gas station north of 94 on Carmichael.



**Anonymous user's Opinion**

Additional manufacturing does not necessarily equate to healthy growth



**Anonymous user's Opinion**

Co-working space for people who work from home or for business incubation



**Anonymous user's Opinion**

- 

**Anonymous user's Opinion**  
Hudson should not become a St.Croix County version of Woodbury. Bring in non-retail business!
- 

**Anonymous user's Opinion**  
Work at home IT with internet including universal Wifi
- 

**Anonymous user's Opinion**  
More medical services. Also a couple of major stores. About the only place to go is east. either north or south of the freeway.
- 

**Anonymous user's Opinion**  
Hudson is the right size, as it is. Stop developing.
- 

**Anonymous user's Opinion**  
We should maintain the unique atmosphere of our river town. I feel we are becoming a little Woodbury. Traffic flow needs more thought, construction of homes is hideous with houses crammed together I.e. golf course. We shop elsewhere because of high prices. I like to support small businesses, but now the downtown parking costs and inconvenience keeps me away. Lots of tax paying boomers now that will become limited in their mobility, etc. progress is necessary but needs to have better goals and planning.
- 

**Anonymous user's Opinion**  
North of Target on the hill (where currently developing by Faith Community) a Kwikrip gas station and decent grocery like HyVee or Festival
- 

**Anonymous user's Opinion**  
We need a community center with an indoor playground and a pool! The space could also have an event center/rooms for rent for events. I thought the golf course area would have been a perfect spot. Now, maybe over where the dog track used to be, or added to Willow River. Hudson kids need a space to swim and play. Hudson is great and I think it is a bit ridiculous we don't already have something like this. We don't need more houses that aren't selling and senior condos.
- 

**Anonymous user's Opinion**  
More apartment buildings that do not have a income restrictions or limits.
- 

**Anonymous user's Opinion**  
Business & Educational training facilities.
- 

**Anonymous user's Opinion**  
Grocery store that offers high quality at reasonable prices
- 

**Anonymous user's Opinion**  
Encourage manufacturing businesses to create jobs.
- 

**Anonymous user's Opinion**  
A children's splash pad near the river for summer play!! With all of our parks, two splash pads would be fantastic- especially as we are attracting young families!
- 

**Anonymous user's Opinion**  
A walk-in, gourmet tacos to-go restaurant in downtown Hudson. A walk-up/ walk-in, to-go creperie in downtown Hudson. They would add diversity to collection of miscellaneous gift shops. Encourage window shopping and can be something quick for people waiting for seating at the other restaurant.
- 

**Anonymous user's Opinion**  
Different grocery store options
- 

**Anonymous user's Opinion**  
New grocery store option to compete with County Market (Hy-Vee, Trader Joe's...) in the old Family Fresh location.
- 

**Anonymous user's Opinion**  
Encourage development away from the river. The new business that build at the end of second street blocks the river view. Push developments up the hill and maintain the downtown look and feel AND stop blocking the river.
- 

**Anonymous user's Opinion**  
Professional businesses that pay a living wage. Don't need any more minimum wage jobs.
- 

**Anonymous user's Opinion**  
The current number of businesses Hudson has is relatively aligned well with the size of the community. There are two consistent themes are here from residents regarding growth/expansion in Hudson: 1. Keep Hudson small 2. We DO NOT want to be like Woodbury.
- 

**Anonymous user's Opinion**  
Put the needs of citizens first. Keep the small town charm and small town amenities for citizens. We are not a big city. We are not the Twin Cities metro, and we are not a suburb. We are a small town of 13,000 people. Decisions should be made for the benefit of the people who currently live here. Making the town bigger by overdevelopment is not the answer. We do not need a Super Target, we do not need a super Walmart. Those big box retailers are only 15 to 20 minutes away in Woodbury, Stillwater, or New Richmond. Encourage small independent businesses, not national chains. As a related note,

just how many banks do we need in a town of this size?



**Anonymous user's Opinion**

No opinion



**Anonymous user's Opinion**

no



**Anonymous user's Opinion**

boutique hotel downtown



**Anonymous user's Opinion**

No known



**Anonymous user's Opinion**

More small business start up assistance to help young or new entrepreneurs develop their ideas. Micro loan programs, business start up education, entrepreneurial support services. It is possible I do not know that this already exists.



**Anonymous user's Opinion**

Opportunity to hire people of all abilities...i.e. people with disabilities.



**Anonymous user's Opinion**

We are building too fast and losing our small town, rural feel. We have a number of vacant strip malls presently, why are more continuing to be built? Also the road infrastructure needs to be considered to handle all the new building traffic.



**Anonymous user's Opinion**

Hudson is grown too big too fast. We do not want to be Woodbury.



**Anonymous user's Opinion**

More local, organic food options in groceries and restaurants. More alternative health facilities including yoga, meditations, acupuncture, etc.



**Anonymous user's Opinion**

Encourage Sustainable resources Discourage Fossil fuel Usage, encourage clean water, sustain healthy river, encourage ways to reduce our carbon footprint.



**Anonymous user's Opinion**

Clean energy and local foods.



**Anonymous user's Opinion**

We need housing for people who work here. Affordable housing. We need public transportation to get people to work, shopping and around town. It saves the roads, reduces need for parking and helps the environment.



**Anonymous user's Opinion**

More FREE parking spaces



**Anonymous user's Opinion**

Technology business could be a boon to Hudson. This requires office buildings that can cater to 20-50 employee offices. 4-6 story buildings with fiber optic internet options are key. Other ecommerce business should be encouraged and advertised. Many of our warehouse spaces could be used for large scale ecommerce that would employ executive to warehouse labor. A 40 Million in sales company generally has 120 employees



**Anonymous user's Opinion**

Parking - downtown



**Anonymous user's Opinion**

Encourage more Hearts Community involvement, convention centers and Parks



**Anonymous user's Opinion**

High tech, engineering, higher paying jobs so everybody doesn't have to drive to the Cities. Anywhere in town would be OK.



**Anonymous user's Opinion**

Unique housing development with green spaces and community. Less "suburban" Woodbury typical subdivisions. Include a section for high quality homes without the large Woodbury McMansions. Extend the quaint downtown community. Keep typical chains in one section -- the hill and restrict expansion. What keeps me in Hudson is everything Woodbury is as a community.



**Anonymous user's Opinion**

I'd like to see public docks for more river traffic to make Hudson a destination. This would bring in more customers for our downtown businesses.



**Anonymous user's Opinion**

Community Center(s) and activities for all ages. Why does this city not have sports programming within its Parks and Recreation Department? This is a huge failing on party of the city to effectively plan growth and attract residents.



**Anonymous user's Opinion**

Water gondola at Lakefront Park. Restaurant at end of dike



**Anonymous user's Opinion**

Hudson should make "waterfront" promotion and development a priority

**Anonymous user's Opinion**



Really love the small business atmosphere - please don't let us become the next big box stop on the freeway. No more big chains! People love the small businesses we have - lets be THAT town. On the outskirts, if we want to stay a "great place to raise a family" some family activity businesses are lacking - we have the SnoPark and outdoor skating, the YMCA skate park, that's great - a waterpark would be nice - like Menomonies - maybe a roller rink, some more in city biking/walking trails, indoor batting cages, mini golf. I'd like to see some additional basic needed goods stores - shoe stores, clothing stores.



**Anonymous user's Opinion**

An interactive city and businesses that promote healthy living and life styles



**Anonymous user's Opinion**

Really miss Family Fresh, 1 grocery store is not enough, now County Market is crazy busy all the time & they run out of stuff.



**Anonymous user's Opinion**

Bring back a downtown grocery store. Bring in Hy-Vee or Trader Joe's to the Family Fresh building.



**Anonymous user's Opinion**

After Econo Foods has closed..need another grocery store. The infracture needs to be looked at before any more housing or industry moves in. The internet is being used up by all the new people and the bandwidth for our area is not keeping up. I am also worried about water and sewers for the area with the amount of people moving in,



**Anonymous user's Opinion**

I think we have plenty of businesses of all sorts in Hudson. I don't think we need any more development. Unfortunately, little by little, all of our green space is being eaten up and many new buildings stand empty. It's ok to have undeveloped open space - that's what makes a town charming.



**Anonymous user's Opinion**

Types of business to discourage is big box. These bring about empty buildings, closed business, added police protection and changes the flavor of the town.



**Anonymous user's Opinion**

Null



**Anonymous user's Opinion**

We would love to see a public mixed-use (kayak, paddle board, rowing) boating facility developed in Hudson. The river and lakes are central to our identity and it would be great to expand water-based recreational options; possibly align with the current public beach area. St Croix Rowing Club is a potential tenant (I am not a member) and would love to see both adult and youth rowing options expanded in our area.



**Anonymous user's Opinion**

I have and will continue to advocate that a segment of our land in the City's Industrial Parks be labeled and identified as a "Technology Park". The IT infrastructure needs to be robust. If we can duplicate what Menomonie has done with their Technology Park, it markets to a cleaner, higher valued type business then the old "industrial" style businesses.



**Anonymous user's Opinion**

Childcare centers clothing boutiques



**Anonymous user's Opinion**

I would differentiate between chain and local restaurants, as well as local retail, ie seasons, grand fete, kelley gallery and target/walmart

**Please indicate to what extent you agree with each of the following statements:**

CURRENT RESULTS	176 Total Responses		
	Agree	Neutral	Disagree
There is an effective level of cooperation between Hudson and its neighboring communities	24% (43)	52% (92)	23% (41)
There is an adequate supply of living wage jobs	20% (35)	38% (67)	42% (74)
Streets in Hudson are generally well maintained	59% (103)	26% (45)	16% (28)
There is a good support system in place for entrepreneurs and new businesses	25% (44)	65% (114)	10% (18)
More bike facilities (bike parking, bike lanes, etc.) are needed within the City	59% (103)	25% (44)	16% (29)
Hudson is a welcoming and attractive place for visitors	73% (128)	20% (36)	7% (12)
City services are effectively provided	52% (91)	37% (65)	11% (20)

## How often do you utilize City of Hudson parks?



## If you use City of Hudson parks, what do you like best about them?

---

-  **Anonymous user's Opinion**  
Beautifully maintained, accessible to all. We need a park on the southern end of town where the new Summit Ridge development is happening. Parks and sidewalks build community.
-  **Anonymous user's Opinion**  
Well maintained.
-  **Anonymous user's Opinion**  
Open spaces.
-  **Anonymous user's Opinion**  
Playgrounds accessible to multiple ages and ability levels
-  **Anonymous user's Opinion**  
Well maintained, not over crowded, beautiful (we generally use Lakefront park and paths around the river.)
-  **Anonymous user's Opinion**  
Well maintained and located throughout city
-  **Anonymous user's Opinion**  
Our family most often visits Lakefront Park, where we enjoy the system of pathways, benches, stage, and venue for community events.
-  **Anonymous user's Opinion**  
Natural beauty. Trails for walking
-  **Anonymous user's Opinion**  
When we used them years ago, it seemed like there were quite a few with broken equipment.
-  **Anonymous user's Opinion**  
waterfront!
-  **Anonymous user's Opinion**  
There access to water.
-  **Anonymous user's Opinion**  
clean, well kept; conveniently located
-  **Anonymous user's Opinion**  
they are appealing and well kept
-  **Anonymous user's Opinion**  
The dog parks and able to get around the river
-  **Anonymous user's Opinion**  
Well kept and all may use free and beautiful. So good to see families having picnics in the park, walkers, fishing. Wonderful atmosphere to keep. Please no more boat mooring to obstruct our beautiful lakefront.
-  **Anonymous user's Opinion**  
Lakefront is both beautiful and functional.
-  **Anonymous user's Opinion**  
Walking paths, playground equipment
-  **Anonymous user's Opinion**  
Open, welcoming, well maintained, accessible to all
-  **Anonymous user's Opinion**  
Well maintained, especially in winter with the sweeping of the paths.

 **Anonymous user's Opinion**  
cleanliness, beauty, facilities

 **Anonymous user's Opinion**  
Views, enjoying nature

 **Anonymous user's Opinion**  
The grass since I live in an apartment

 **Anonymous user's Opinion**  
They are well maintained and a great source of free entertainment for my children.

 **Anonymous user's Opinion**  
The trees and landscaping, playgrounds for the kids, Picnic areas, and that dogs are welcome.

 **Anonymous user's Opinion**  
Well maintained.

 **Anonymous user's Opinion**  
Don't use them

 **Anonymous user's Opinion**  
Convenient locations and adequate facilities.

 **Anonymous user's Opinion**  
Easy access (parking), quiet, well-maintained and lit (safe)

 **Anonymous user's Opinion**  
The walking trails

 **Anonymous user's Opinion**  
The playground equipment for the children. I would love to see equipment for older children too. I also like the walking paths too!

 **Anonymous user's Opinion**  
Access to the water through beaches and the river.

 **Anonymous user's Opinion**  
They are well maintained

 **Anonymous user's Opinion**  
Love the equipment and ability for kids to safely play

 **Anonymous user's Opinion**  
I like that they are kept very clean

 **Anonymous user's Opinion**  
They're generally clean and safe/appropriate for children of all ages.

 **Anonymous user's Opinion**  
Clean

 **Anonymous user's Opinion**  
Cleanliness

 **Anonymous user's Opinion**  
Proximity; I live across from a park and there are several within walking distance. My kids used to use the ice rinks a few blocks away too. I also frequent the dog park.

 **Anonymous user's Opinion**  
Location and accessibility.

 **Anonymous user's Opinion**  
Located on the river.

**Anonymous user's Opinion**



Usually good amenities, fairly clean



**Anonymous user's Opinion**

Trees, flowers, things for all ages to do



**Anonymous user's Opinion**

Don't use



**Anonymous user's Opinion**

Clean and well maintained. Sense of safety is very important to maintain. I would suggest playground equipment should be inspected/fixed at least annually.



**Anonymous user's Opinion**

General cleanliness



**Anonymous user's Opinion**

locations



**Anonymous user's Opinion**

Clean and well maintained



**Anonymous user's Opinion**

Walking paths, how the City keeps the parks maintained and clean.



**Anonymous user's Opinion**

well maintained



**Anonymous user's Opinion**

birkmose park, i like the view



**Anonymous user's Opinion**

Kid friendly, community based, and well stocked with equipment.



**Anonymous user's Opinion**

Doesn't apply



**Anonymous user's Opinion**

They are beautiful and easy to access. Not a fan of the parking idea downtown. Makes me not want to go to the bandshell or the dike.



**Anonymous user's Opinion**

Adjacent to the river



**Anonymous user's Opinion**

Great facilities for children and adults.



**Anonymous user's Opinion**

Ease of access, well maintained



**Anonymous user's Opinion**

Kept natural.



**Anonymous user's Opinion**

The parks are well maintained.



**Anonymous user's Opinion**

Greenspace w/ mature trees. the last thing needed here is MORE businesses, housing and concrete. Turn any existing open land into greenspace (parks etc0



**Anonymous user's Opinion**

The location.



**Anonymous user's Opinion**

Proximity and wild spaces. i.e. area north and west of the YMCA.

**Anonymous user's Opinion**

-  Cleanliness, well maintained.
-  **Anonymous user's Opinion**  
Leave them as is
-  **Anonymous user's Opinion**  
Accessible
-  **Anonymous user's Opinion**  
Nice place for kids to have fun for free and a good place to meet neighbors.
-  **Anonymous user's Opinion**  
Lots of parking, safe, good trails to walk.
-  **Anonymous user's Opinion**  
Well Lakefront park is beautiful and has lot of good walking. Other parks the kids play equipment.
-  **Anonymous user's Opinion**  
I use to like Lake Front, but not anymore. Way too much traffic. Prospect Park is now a go too
-  **Anonymous user's Opinion**  
A place to play pickle ball
-  **Anonymous user's Opinion**  
Grilling facilities Dog friendly Well maintained (landscaping) Easy to coordinate renting space (Thank you Amanda Roy)
-  **Anonymous user's Opinion**  
I don't really use them so have no answer - I guess I love the dog park, but that's not technically a city park, even though it's on city property.
-  **Anonymous user's Opinion**  
The primary area I use is the water front. I believe Hudson developed Lakefront in a way that maximizes this asset.
-  **Anonymous user's Opinion**  
Clean
-  **Anonymous user's Opinion**  
The walking paths.
-  **Anonymous user's Opinion**  
Use more often in summer, less in winter. Like accessibility to Lakefront Park, our #1 park destination. Like proximity to St. Croix River and site lines, park walk out to river and from RR tracks south the the boat launch and marina.
-  **Anonymous user's Opinion**  
Parking (although it used to be free on weekends and paid parking downtown will probably discourage people from using Lakefront Park), cleanliness, the view.
-  **Anonymous user's Opinion**  
Abundance of them and how close they are to neighborhoods. We do need more recreation options closer to the namekagon neighborhood and low income housing for those kids to have something safe to do within biking distance.
-  **Anonymous user's Opinion**  
open green space
-  **Anonymous user's Opinion**  
Well maintained from what I've experienced. I do use the small dog park which is so nice to have that option for a small dog.
-  **Anonymous user's Opinion**  
I like that they are clean and fairly well maintained.
-  **Anonymous user's Opinion**  
The grounds are well-maintained.
-  **Anonymous user's Opinion**  
Always well maintained



**Anonymous user's Opinion**

mix of play grounds, open space, paved trails for biking.



**Anonymous user's Opinion**

Playground equipment is in good condition



**Anonymous user's Opinion**

Safe, scenic, clean. Absolutely love walking the dike road and the improvements that have already been done.



**Anonymous user's Opinion**

Clean and well kept



**Anonymous user's Opinion**

Geocaching



**Anonymous user's Opinion**

Wide open space and not crowded



**Anonymous user's Opinion**

Clean



**Anonymous user's Opinion**

Variety.



**Anonymous user's Opinion**

Well cared for grounds and facilities. Good landscaping and greenery (trees)



**Anonymous user's Opinion**

Water access and view



**Anonymous user's Opinion**

clean



**Anonymous user's Opinion**

Clean, well maintained, love Lakefront Park. But need to be able to use it without paying for parking.



**Anonymous user's Opinion**

Distance to river.



**Anonymous user's Opinion**

Playground structures, clean areas.



**Anonymous user's Opinion**

They're not overcrowded or destroyed by people from outside the community



**Anonymous user's Opinion**

I like most of them, but the riverfront is becoming trashy. We should be thinking about maintaining its beauty, not how to build it up.



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

Clean and well maintained



**Anonymous user's Opinion**

Hiking trails, used more before the dam repairs (I live near Willow) exercise



**Anonymous user's Opinion**

Clean, accessible, Willow River is awesome



**Anonymous user's Opinion**

Clean, open, easy parking, lot of space.

**Anonymous user's Opinion**



Proximity to river



**Anonymous user's Opinion**

Close proximity to St. Croix river.



**Anonymous user's Opinion**

Openness



**Anonymous user's Opinion**

Options and maintenance



**Anonymous user's Opinion**

Clean. Well maintained.



**Anonymous user's Opinion**

Usually open and clean



**Anonymous user's Opinion**

convenient and well kept.



**Anonymous user's Opinion**

Variety, clean, no two parks are the same



**Anonymous user's Opinion**

E perhaps additional exercise based playground like the one downtown by the riverach park has a different types of play equipment. That means our kids like all of them for different reasons.



**Anonymous user's Opinion**

Cleanliness, variety of playgrounds, recreation



**Anonymous user's Opinion**

Quality of playground equipment



**Anonymous user's Opinion**

Open space.



**Anonymous user's Opinion**

The river front, trails and the beach



**Anonymous user's Opinion**

Mostly to walk and enjoy the weather.



**Anonymous user's Opinion**

.



**Anonymous user's Opinion**

They are not overcrowded. That is with the exception of Willow River Park. I no longer go there as it is overrun with people from the twin cities.



**Anonymous user's Opinion**

They are fairly well equipped



**Anonymous user's Opinion**

Well marked directions



**Anonymous user's Opinion**

Clean and well maintained



**Anonymous user's Opinion**

Clean and attractive



**Anonymous user's Opinion**

Well maintained, not overcrowded, variety

**Anonymous user's Opinion**



I'm not a winter person so use them only in summer and usually only with grandson. Don't have any opinion.



**Anonymous user's Opinion**

Locations, cleanliness, walking paths



**Anonymous user's Opinion**

I take my grandkids and we enjoy the facilities for the kids, playgrounds and equipment.



**Anonymous user's Opinion**

Dog bags are provided and the lighted path by the river is great.



**Anonymous user's Opinion**

I don't use them anymore



**Anonymous user's Opinion**

Access



**Anonymous user's Opinion**

NA



**Anonymous user's Opinion**

Nice play equipment for families Sports fields The newer pathway along the river front area



**Anonymous user's Opinion**

They are clean and have fun spaces for kids



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

I like the trails in the woods, easy to access, they seem clean



**Anonymous user's Opinion**

Along the river



**Anonymous user's Opinion**

I enjoy walking through Lakefront Park in particular. I like the multi-use factor of it.



**Anonymous user's Opinion**

Trees, walking paths, view along the river, places to sit or picnic.



**Anonymous user's Opinion**

pick up drift wood



**Anonymous user's Opinion**

They are calm and relaxing. Our state park is much better than our city parks though.



**Anonymous user's Opinion**

The open space and the cleanliness



**Anonymous user's Opinion**

Clean, well groomed



**Anonymous user's Opinion**

green space



**Anonymous user's Opinion**

Wooded and undeveloped Open Spaces



**Anonymous user's Opinion**

Walking paths, access to river

**Anonymous user's Opinion**



Walking paths and exercise opportunities.



**Anonymous user's Opinion**

Well maintained and scenic.



**Anonymous user's Opinion**

clean well maintained and variety



**Anonymous user's Opinion**

I enjoy walking along the paths in Lakefront Park, and the Dike Road.



**Anonymous user's Opinion**

well maintained clean



**Anonymous user's Opinion**

Restrict pets.



**Anonymous user's Opinion**

Working bathrooms and within walking distance or adequate parking spaces



**Anonymous user's Opinion**

Attractive, Clean, most are updated and well maintained



**Anonymous user's Opinion**

Views of river



**Anonymous user's Opinion**

The waterfront, although it could use modernization and upgrading



**Anonymous user's Opinion**

Not too crowded. Always a parking area. Seem to be well maintained.



**Anonymous user's Opinion**

Maintained



**Anonymous user's Opinion**

Clean, well maintained, safe



**Anonymous user's Opinion**

North Hudson parks are well maintained and promote wildlife and native plants.



**Anonymous user's Opinion**

Lakefront Park, walking and watching the sunsets.



**Anonymous user's Opinion**

Ease of parking



**Anonymous user's Opinion**

For the most part, they are well-maintained and clean. Baseball diamonds just outside of town in Troy township and Town Hall could be better maintained.



**Anonymous user's Opinion**

I like the amount and variety of city park. Lakefront is good for walks, Grandview has a nice playground for the kids, Deer Run is small and fairly private.



**Anonymous user's Opinion**

Green space



**Anonymous user's Opinion**

use very little



**Anonymous user's Opinion**

I like the exercise stations near lakefront.

**Anonymous user's Opinion**

-  Love the lakefront and dike road and path along the river. Great for walking or strolling. Love the exercise equip there. Love the band shell and events. Would like it to remain an event center and a walk along the river. Safe and quiet and still somewhat a local enjoyment.
-  **Anonymous user's Opinion**  
Clean, spacious
-  **Anonymous user's Opinion**  
convenient locations, unique features, well maintained.
-  **Anonymous user's Opinion**  
They are clean, accessible, safe, and well-maintained. We love that they are welcoming and inclusive of individuals of all abilities.
-  **Anonymous user's Opinion**  
I don't use them
-  **Anonymous user's Opinion**  
They are clean, safe, and well maintained.
-  **Anonymous user's Opinion**  
I like play structures for young children, sledding hills, city beaches. I like the location and proximity to home.
-  **Anonymous user's Opinion**  
beauty, varied locations throughout the city

## If you don't use City of Hudson parks, why not?

---



Anonymous user's Opinion

I do use them!



Anonymous user's Opinion

I do use them!



Anonymous user's Opinion

I use them.



Anonymous user's Opinion

n/a



Anonymous user's Opinion

Not aware of any activities or other uses for any other parks in the city. Also, my kids are grown.



Anonymous user's Opinion

NA, I use Hudson Parks often.



Anonymous user's Opinion

NA



Anonymous user's Opinion

I use them! Not applicable



Anonymous user's Opinion

My kids are not an ages where they use parks for playing or for sports.



Anonymous user's Opinion

I do!



Anonymous user's Opinion

Where can one park? Often we just drive on through as we note that there's something going on but nowhere to park. Why not shuttles?



Anonymous user's Opinion

I use the parks



Anonymous user's Opinion

do not walk much



Anonymous user's Opinion

We use them daily



Anonymous user's Opinion

I do use



Anonymous user's Opinion

I use them mostly for walks.



Anonymous user's Opinion

I use parks



Anonymous user's Opinion

I use the parks



Anonymous user's Opinion

Use them

 Anonymous user's Opinion

I do use the parks

 Anonymous user's Opinion

I don't have enough time, or there are many in my location.

 Anonymous user's Opinion

N/A

 Anonymous user's Opinion

I use them

 Anonymous user's Opinion

I do use them.

 Anonymous user's Opinion

N/A

 Anonymous user's Opinion

I use parks

 Anonymous user's Opinion

nothing to offer

 Anonymous user's Opinion

N/A

 Anonymous user's Opinion

I use them

 Anonymous user's Opinion

I currently use them.

 Anonymous user's Opinion

n/a

 Anonymous user's Opinion

I have trouble walking on uneven ground

 Anonymous user's Opinion

NA

 Anonymous user's Opinion

Na

 Anonymous user's Opinion

We use a lot of the city's parks. I wish there were more bike paths to get to the parks.

 Anonymous user's Opinion

Na

 Anonymous user's Opinion

N/A

 Anonymous user's Opinion

Not applicable

 Anonymous user's Opinion

I DO use the parks.

 Anonymous user's Opinion

Parking can be difficult. Not much to do at some parks unless you have little kids.

Anonymous user's Opinion



n/a



**Anonymous user's Opinion**

Grown children



**Anonymous user's Opinion**

No time



**Anonymous user's Opinion**

I use Hudson Parks.



**Anonymous user's Opinion**

N/a



**Anonymous user's Opinion**

We use them



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

See above, I use the parks.



**Anonymous user's Opinion**

I use them



**Anonymous user's Opinion**

i use birkmose



**Anonymous user's Opinion**

We do use them, see question 8. 2-3 times or more per month depending on weather and schedules.



**Anonymous user's Opinion**

No current need for them (personally)



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

I do use them



**Anonymous user's Opinion**

I do use city parks.



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

Needs better maintenances



**Anonymous user's Opinion**

There is an overemphasis on active recreational uses.



**Anonymous user's Opinion**

I plan to buy a State park pass next year, this year it was too late in the season when I got around to it.



**Anonymous user's Opinion**

Use the parks



**Anonymous user's Opinion**

Limited children's play structures does limit use of some parks.

**Anonymous user's Opinion**



Lack of close parking



Anonymous user's Opinion

N/A



Anonymous user's Opinion

I use them.



Anonymous user's Opinion

I use the city parks for weekly physical education and birthday parties for my kids.



Anonymous user's Opinion

I do.



Anonymous user's Opinion

NA



Anonymous user's Opinion

I do on occasion.



Anonymous user's Opinion

NA



Anonymous user's Opinion

I do use them.



Anonymous user's Opinion

I like them there for the people that do use them and for the city to keep green space. I usually take my "park" time either at home with the dogs or at our camper.



Anonymous user's Opinion

I don't use the other parks because of proximity to where I live.



Anonymous user's Opinion

I do



Anonymous user's Opinion

I use city parks.



Anonymous user's Opinion

We do visit Birkmose Park now and then for its long-distance vistas and relative safety. It could use some attention. Seldom visit Weitkamp Park or any of the spaces for walking dogs.



Anonymous user's Opinion

Probably won't use Lakefront Park as much because the fees/hours for parking have changed



Anonymous user's Opinion

I do use them.



Anonymous user's Opinion

I use the parks



Anonymous user's Opinion

N/A



Anonymous user's Opinion

Our kids are high school she so we do not frequent them like we used to.



Anonymous user's Opinion

We don't have children or grand-children who live in the area, so am usually only there for family events like graduation parties and the like.



Anonymous user's Opinion

We use them on a frequent basis

Anonymous user's Opinion



We do.



Anonymous user's Opinion

NA



Anonymous user's Opinion

n/a



Anonymous user's Opinion

Na



Anonymous user's Opinion

Too busy.



Anonymous user's Opinion

N/a



Anonymous user's Opinion

No time



Anonymous user's Opinion

na



Anonymous user's Opinion

N/A



Anonymous user's Opinion

I use the parks



Anonymous user's Opinion

nothing there for me



Anonymous user's Opinion

I do use the parks. See question 8. Something is wrong with your software.



Anonymous user's Opinion

We do use the parks.



Anonymous user's Opinion

Could use better trails to connect Hudson from south or north of I94 (overpasses?)



Anonymous user's Opinion

I do use the parks.



Anonymous user's Opinion

No need



Anonymous user's Opinion

I do



Anonymous user's Opinion

Health reasons recently



Anonymous user's Opinion

NA



Anonymous user's Opinion

do not have time.



Anonymous user's Opinion

N/A

Anonymous user's Opinion

 I do frequent the City of Hudson parks, especially the park by the St. Croix river walk.

 **Anonymous user's Opinion**

NA

 **Anonymous user's Opinion**

I use them and so do my children.

 **Anonymous user's Opinion**

Have not had a need, children out of the house.

 **Anonymous user's Opinion**

We use the parks.

 **Anonymous user's Opinion**

Sometimes closed.

 **Anonymous user's Opinion**

I use them.

 **Anonymous user's Opinion**

I use the parks

 **Anonymous user's Opinion**

We do.

 **Anonymous user's Opinion**

Sometimes too cold outside for little kids

 **Anonymous user's Opinion**

N/a

 **Anonymous user's Opinion**

I use the parks

 **Anonymous user's Opinion**

I do use them

 **Anonymous user's Opinion**

na

 **Anonymous user's Opinion**

NA

 **Anonymous user's Opinion**

Na

 **Anonymous user's Opinion**

I use the city parks.

 **Anonymous user's Opinion**

No reason

 **Anonymous user's Opinion**

We use the parks

 **Anonymous user's Opinion**

Would use them more often if they had more picnic facilities, such as picnic tables, public grills, and covered Pavilion type structures.

 **Anonymous user's Opinion**

I do use them

**Anonymous user's Opinion**



Not much leisure time.



**Anonymous user's Opinion**

n/a



**Anonymous user's Opinion**

I use them.



**Anonymous user's Opinion**

NA



**Anonymous user's Opinion**

I used to love them and bringing my child, but now they're all adult filled Pokémon stops and irresponsible pet owners that don't pick up after their dog.



**Anonymous user's Opinion**

I use the parks.



**Anonymous user's Opinion**

None near my house and don't want to drive to a park



**Anonymous user's Opinion**

See answer above



**Anonymous user's Opinion**

I do



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

N/a



**Anonymous user's Opinion**

No trails, seem like they are set up for picnicking not enjoyment of being outdoors.



**Anonymous user's Opinion**

n/a



**Anonymous user's Opinion**

But I do use the parks,



**Anonymous user's Opinion**

I live in River Falls



**Anonymous user's Opinion**

I do use hudson parks.



**Anonymous user's Opinion**

I get outdoor exercise in my neighborhood and indoor exercise at the "Y"



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

N/A



**Anonymous user's Opinion**

We all use them.



**Anonymous user's Opinion**

We do use them

**Anonymous user's Opinion**

-  I do use the parks.
-  **Anonymous user's Opinion**  
I do use the parks a lot.
-  **Anonymous user's Opinion**  
N/A
-  **Anonymous user's Opinion**  
I think most of the city parks are geared to families with children - playgrounds, etc. Instead, I visit Willow River park almost daily to get a hike in. I would prefer to stay closer to home, however.
-  **Anonymous user's Opinion**  
I use city parks
-  **Anonymous user's Opinion**  
Insufficient water and restroom facilities.
-  **Anonymous user's Opinion**  
I use them
-  **Anonymous user's Opinion**  
I do use the parks
-  **Anonymous user's Opinion**  
I do use City of Hudson parks
-  **Anonymous user's Opinion**  
I use them!
-  **Anonymous user's Opinion**  
Just moved here
-  **Anonymous user's Opinion**  
I used to when our kids were small - all the time. Went to Williams Park almost daily. Grandview when it opened, Lakefront (except when the convicts were there picking up trash). But the parks are geared towards kids. I'd love to see somemore open space/trail type areas IN the city to just go on nice walks, dog walks or bike rides.
-  **Anonymous user's Opinion**  
Kids are grown
-  **Anonymous user's Opinion**  
I live in North Hudson.
-  **Anonymous user's Opinion**  
I would love to see tennis and pickle ball courts that are public, also a pool or water park for kids would be a much safer option than open public beaches on the river.
-  **Anonymous user's Opinion**  
—
-  **Anonymous user's Opinion**  
My kids are getting older, so we don't use the parks as much as we used to do.
-  **Anonymous user's Opinion**  
I use city parks.
-  **Anonymous user's Opinion**  
My kids are older. I mostly prefer the state park.
-  **Anonymous user's Opinion**  
too many people and not well maintained.
-  **Anonymous user's Opinion**  
I do use them.
-  **Anonymous user's Opinion**

-  Don't use some parks as they are geared toward children and past that! I think the value of the other parks are great. Gives kids places to play ball. Love the fact that we have an undeveloped park- Burton field that allows kids to pick up and play ball, run, frisbee, and skate! One of the best treasures.
-  **Anonymous user's Opinion**  
Null
-  **Anonymous user's Opinion**  
disconnected from trails and sidewalks.
-  **Anonymous user's Opinion**  
N/A
-  **Anonymous user's Opinion**  
just not my thing to go hang out in a city park
-  **Anonymous user's Opinion**  
I do use the parks.
-  **Anonymous user's Opinion**  
na
-  **Anonymous user's Opinion**  
Distance from my home and ability to access if I choose not to drive.

## What would you like to see added to / changed with City of Hudson parks?

---

-  **Anonymous user's Opinion**  
Add a park to the southern development across from the dog track. Build more walking paths.
-  **Anonymous user's Opinion**  
Need more Pickleball courts. Need more connected off road bike paths.
-  **Anonymous user's Opinion**  
Better parking around some of them.
-  **Anonymous user's Opinion**  
continue to maintain equipment safety
-  **Anonymous user's Opinion**  
I would love to see an outdoor pool and/or water park for families! This has been needed for decades.
-  **Anonymous user's Opinion**  
Increase accessibility of all parks
-  **Anonymous user's Opinion**  
Hold the Hudson Farmers' Market at Lakefront Park.
-  **Anonymous user's Opinion**  
better public fishing access
-  **Anonymous user's Opinion**  
1. We need more parks with more variety. Places for picnics and playing. We need a park with some serious amenities like a waterpark operated by the city. 2. We could use a municipal Park and Rec department that could run children's sports activities. We are so lacking the a cohesive plan on sports for youngsters. There's overlap and disorganization. Some association sports groups suffer from the egos of parents who have too much control and too many unfulfilled athletic dreams. People who move here or have lived other places see how patchwork and underwhelming our system is. It's embarrassing.
-  **Anonymous user's Opinion**  
City docks
-  **Anonymous user's Opinion**  
Ambulatory food sales.
-  **Anonymous user's Opinion**  
N/A
-  **Anonymous user's Opinion**  
a public pool
-  **Anonymous user's Opinion**  
More of them
-  **Anonymous user's Opinion**  
Nothing!
-  **Anonymous user's Opinion**  
Get the land east of Grandview and north of the YMCA purchased and reserved for a park.
-  **Anonymous user's Opinion**  
Better upkeep of playground by the Marina on the St. Croix River
-  **Anonymous user's Opinion**  
I love the parks
-  **Anonymous user's Opinion**  
Seem ok

-  **Anonymous user's Opinion**  
Nothing
-  **Anonymous user's Opinion**  
I think they are nice now. I bike path or walking path south of town, down Carmichael/F would be great!
-  **Anonymous user's Opinion**  
More seating options either benches or tables
-  **Anonymous user's Opinion**  
It would be cool to have a splash pad at one of the parks.
-  **Anonymous user's Opinion**  
Bathroom facilities open past Labor Day.
-  **Anonymous user's Opinion**  
Walking trails and sidewalks and mark them more with overall plan published with signage.
-  **Anonymous user's Opinion**  
don't care, but don't spend my tax dollars stupidly on parks
-  **Anonymous user's Opinion**  
Lakefront could be better utilized and maintained. That resource should be a key feature of the town and it looks like an afterthought.
-  **Anonymous user's Opinion**  
Extensive biking paths and extended ski paths
-  **Anonymous user's Opinion**  
More walking trails
-  **Anonymous user's Opinion**  
Add newer equipment at some of the parks. It would be nice to have some play equipment (swings, slides) near the downtown beach for variety at the beach. It would be nice to have a little splash pad (water spouts) at Weitkamp park. It would be within the handicap accessible framework and a great addition to the park to cool off the children with no lifeguard needed.
-  **Anonymous user's Opinion**  
More control of animals and requiring owners to clean up after them.
-  **Anonymous user's Opinion**  
Seating
-  **Anonymous user's Opinion**  
Sidewalks and accessibility. Williams park doesn't have any sidewalks leading up to it, making it very hard to navigate my stroller and safely walk there.
-  **Anonymous user's Opinion**  
Na
-  **Anonymous user's Opinion**  
Please add more bike paths and bike racks.
-  **Anonymous user's Opinion**  
Na
-  **Anonymous user's Opinion**  
Add restroom facilities
-  **Anonymous user's Opinion**  
A full-sized outdoor basketball court somewhere downtown would be nice.
-  **Anonymous user's Opinion**  
More walking trails.
-  **Anonymous user's Opinion**  
More natural - not so groomed (trees removed, etc)

 Anonymous user's Opinion

Bike/walking trails.

 Anonymous user's Opinion

Would love a community pool

 Anonymous user's Opinion

Nothing

 Anonymous user's Opinion

Additional police presence at the end of the Pier to ensure public behavior is maintained.

 Anonymous user's Opinion

More bike/walking paths

 Anonymous user's Opinion

irragation and weed control

 Anonymous user's Opinion

More restrooms

 Anonymous user's Opinion

More flowers, additional trash receptacles at the smaller parks. Something to add more interest at the dog parks.

 Anonymous user's Opinion

no problems I see

 Anonymous user's Opinion

Utilize unused spaces / grass areas for pollinator habitat.

 Anonymous user's Opinion

More climbing ropes and monkey bars like the old days. Not holding my breath here.

 Anonymous user's Opinion

Doesn't apply

 Anonymous user's Opinion

More trees for hammocks

 Anonymous user's Opinion

Nothing

 Anonymous user's Opinion

More summertime activities at Lakefront Park.

 Anonymous user's Opinion

No current thoughts on this.

 Anonymous user's Opinion

Better maintenances

 Anonymous user's Opinion

Greater focus on restoring natural habitats and reducing the quantity of grass in areas that are not frequented by visitors.

 Anonymous user's Opinion

More of them. Fewer new businesses, apartment buildings and houses.

 Anonymous user's Opinion

Splash pad/community pool. Parking

 Anonymous user's Opinion

More child areas. A dog friendly area along the river front for water dogs.

 Anonymous user's Opinion

-  More green space, more use of the rivers edge (access).
-  **Anonymous user's Opinion**  
Nothing
-  **Anonymous user's Opinion**  
Better up keep.
-  **Anonymous user's Opinion**  
None
-  **Anonymous user's Opinion**  
Splash pad in summer. Continue to maintain kids playgrounds.
-  **Anonymous user's Opinion**  
It is so difficult for older people to go to these parks. They cannot walk far, so parking is an issue. They need to sit, so more seating.
-  **Anonymous user's Opinion**  
Additional pickle ball courts
-  **Anonymous user's Opinion**  
Lights Dog bag stations Handicapped options for play areas
-  **Anonymous user's Opinion**  
I have not considered the dog park in these responses, but that one I use almost daily. It would be great to see improvements there, but I understand that is not the city's responsibility.
-  **Anonymous user's Opinion**  
More nature walks.
-  **Anonymous user's Opinion**  
Unsure
-  **Anonymous user's Opinion**  
More grills
-  **Anonymous user's Opinion**  
Pedestrian access throughout the city to connect to parks.
-  **Anonymous user's Opinion**  
Just make sure they are wheelchair accessible. The mini-park on the path just north of the beach pavilion at Lakefront Park has workout equipment some of which can be used from a wheelchair. Many thanks.
-  **Anonymous user's Opinion**  
More open spaces, more accessibility
-  **Anonymous user's Opinion**  
More parks added to low income housing areas
-  **Anonymous user's Opinion**  
More walking paths installed
-  **Anonymous user's Opinion**  
I don't have a suggestion.
-  **Anonymous user's Opinion**  
We would love to see a bike park or more mountain biking trails. Larger parks with ski trails as well.
-  **Anonymous user's Opinion**  
That the facilities within the parks aren't being taken over by vagrants.
-  **Anonymous user's Opinion**  
Just more of them

-  **Anonymous user's Opinion**  
more walking/biking trails
-  **Anonymous user's Opinion**  
More sports facilities; basketball, disc golf, horseshoes, etc
-  **Anonymous user's Opinion**  
Cannot think of anything at this time.
-  **Anonymous user's Opinion**  
Na
-  **Anonymous user's Opinion**  
Nothing
-  **Anonymous user's Opinion**  
Nothing
-  **Anonymous user's Opinion**  
Local and historical facts
-  **Anonymous user's Opinion**  
more bathrooms
-  **Anonymous user's Opinion**  
More natural areas, and pollinator friendly plantings
-  **Anonymous user's Opinion**  
Support the current Park Master plan proposal regarding a full length Mooring Field Dock. Provided that the Mooring Access Dinghy's may be kept and chained by each member to the North side of the dock. Mooring Field access by sliding the light weight dinghy's over the dock as needed. Our experience with the new rock on the Diked Road have made stairway access too hazardous.
-  **Anonymous user's Opinion**  
sidewalks to walk to the parks
-  **Anonymous user's Opinion**  
Additional shore fishing opportunities along the St. Croix River.
-  **Anonymous user's Opinion**  
Nothing.
-  **Anonymous user's Opinion**  
Better facilities for bathrooms/infant changing areas.
-  **Anonymous user's Opinion**  
Enforced restrictions on vagrant camping
-  **Anonymous user's Opinion**  
Answered above.
-  **Anonymous user's Opinion**  
More paths
-  **Anonymous user's Opinion**  
N/A
-  **Anonymous user's Opinion**  
expanded trails, community center and pool added somewhere
-  **Anonymous user's Opinion**  
more boat related things at lakefront
-  **Anonymous user's Opinion**  
More restrooms

-  **Anonymous user's Opinion**  
Walking, jogging, & hiking paths on east side of St. Croix river north of I-94.
-  **Anonymous user's Opinion**  
Natural look
-  **Anonymous user's Opinion**  
Nothing at this time
-  **Anonymous user's Opinion**  
N/A
-  **Anonymous user's Opinion**  
Neutral
-  **Anonymous user's Opinion**  
Pretty good as they are.
-  **Anonymous user's Opinion**  
DOG PARK DOWNTOWN, restrooms, and concessions at parks downtown.
-  **Anonymous user's Opinion**  
Splash pads for summer, shaded spots, more picnic tables
-  **Anonymous user's Opinion**  
Perhaps the addition of another adult exercise playground like the one that is across from the public library.
-  **Anonymous user's Opinion**  
Drinking fountains that work well, portable and maintained restrooms, more outdoor recreation at the park by the middle school such as tennis or pickle ball
-  **Anonymous user's Opinion**  
Community swimming pool.
-  **Anonymous user's Opinion**  
More walking paths.
-  **Anonymous user's Opinion**  
More use of the dike road (art fairs, car shows, music, etc)
-  **Anonymous user's Opinion**  
na
-  **Anonymous user's Opinion**  
Dog leash laws enforced and dog feces pickup by dog owners. Burton field smells horrid, especially after freeze thaw.
-  **Anonymous user's Opinion**  
NA
-  **Anonymous user's Opinion**  
More shelters if they can be maintained.
-  **Anonymous user's Opinion**  
None
-  **Anonymous user's Opinion**  
Nothing at this time they seem to be well maintained and clean
-  **Anonymous user's Opinion**  
See my answer to question 10. Also, we would use Lakefront Park more often if there was more free parking. In fact, we would frequent the businesses downtown more if the parking were free like it is in River Falls and New Richmond.
-  **Anonymous user's Opinion**  
No opinion

-  **Anonymous user's Opinion**  
Would like to see a bigger more modern playground for older children. It is very difficult when very young and very old children use the same one at the same time.
-  **Anonymous user's Opinion**  
????
-  **Anonymous user's Opinion**  
A big public swimming pool.
-  **Anonymous user's Opinion**  
More bike and walking paths.
-  **Anonymous user's Opinion**  
Open bathrooms. But it's been ruined by irresponsible people.
-  **Anonymous user's Opinion**  
More handicapped spaces and an adult changing table in a few of the parks.
-  **Anonymous user's Opinion**  
NA
-  **Anonymous user's Opinion**  
I would like more pickleball courts and a dog park designated for small dogs.
-  **Anonymous user's Opinion**  
The parks are fine!
-  **Anonymous user's Opinion**  
N/A
-  **Anonymous user's Opinion**  
More signage to show trails and easier to find
-  **Anonymous user's Opinion**  
More places to sit, more trails, more trees, more opportunities for interacting with nature.
-  **Anonymous user's Opinion**  
More native plantings and more public art.
-  **Anonymous user's Opinion**  
No ideas.
-  **Anonymous user's Opinion**  
sidewalk from river parking lot to street so I don't have to climb across the slippery mud or grass.
-  **Anonymous user's Opinion**  
Mountain bike trails at Birkmose Park created. Currently there are over 150 youth and parents on the St Croix Valley Mountain biking team. They would be willing to create and maintain sustainable trails at this location. Hudson currently has no mountain biking available.
-  **Anonymous user's Opinion**  
Nothing, really
-  **Anonymous user's Opinion**  
More and interconnected network
-  **Anonymous user's Opinion**  
additional parking downtown
-  **Anonymous user's Opinion**  
More community activities or businesses that encourage nature awareness and the environment.
-  **Anonymous user's Opinion**  
More connected longer walking paths, bike paths, Canoe rack at River

-  **Anonymous user's Opinion**  
A bike trail.
-  **Anonymous user's Opinion**  
Nothing. The parks are fine.
-  **Anonymous user's Opinion**  
maintain equipment and replace when necessary
-  **Anonymous user's Opinion**  
I think we are becoming a much more health conscious society, but the city lacks adult amenities like walking/running trails and paths. I would like to see the city invest more in well-lit, paved trails that are maintained for year round use.
-  **Anonymous user's Opinion**  
Increase flowers and colorful planting
-  **Anonymous user's Opinion**  
More drinking fountains.
-  **Anonymous user's Opinion**  
Working bathrooms in all parks
-  **Anonymous user's Opinion**  
There needs to be more connection points, between parks, neighborhood, and the rest of the community. It is difficult and often unsafe as a pedestrian or cyclist to get from one area or neighborhood to another within our community. As stated earlier, the city lacks in planning for recreation adequately.
-  **Anonymous user's Opinion**  
More trees and shrubs with less lawn
-  **Anonymous user's Opinion**  
More emphasis on the waterfront and connection of Hudson to it as the city has grown.
-  **Anonymous user's Opinion**  
Bike and walking lanes
-  **Anonymous user's Opinion**  
I would like to see more neighborhood parks - even if they are smaller - we can't let every square inch be developed. Some open space with trails - a park doesn't have to mean swingset. More parks would be awesome. Also always wanted an outdoor community swimming pool.
-  **Anonymous user's Opinion**  
Nothing
-  **Anonymous user's Opinion**  
Increased free parking.
-  **Anonymous user's Opinion**  
Tennis courts, pool/water park for kids
-  **Anonymous user's Opinion**  
Newer equipment
-  **Anonymous user's Opinion**  
Updated restrooms and water fountains.
-  **Anonymous user's Opinion**  
Outdoor basketball courts, public skate-park, tie in of informative signage on Hudson history.
-  **Anonymous user's Opinion**  
More walking trails
-  **Anonymous user's Opinion**  
Easy parking
-  **Anonymous user's Opinion**  
Better trail system from park to park.



**Anonymous user's Opinion**

I think that facilities such as bathrooms to some of the parks would be a great addition! Such as Prospect and Burton.



**Anonymous user's Opinion**

Parking



**Anonymous user's Opinion**

Enhance existing spaces, continued maintenance. Need for additional parking added or addressed at Grandview with softball, baseball and dog parks all sharing current lot.



**Anonymous user's Opinion**

An outdoor public pool would be great.



**Anonymous user's Opinion**

More walking and biking paths



**Anonymous user's Opinion**

I would personally like to see public Tennis Courts. The only public courts in the city of Hudson are the Hudson High School tennis courts and when they are being utilized by the School District, there are NO public tennis courts. They even charge the Hudson Tennis Association a fee to use the courts for organized activities, when the HTA would like to reserve them for kid's activities!



**Anonymous user's Opinion**

more neighborhood parks



**Anonymous user's Opinion**

access from all areas of the community by walking or riding. interconnected by walking biking paths

**Please indicate if you agree or disagree with the following statements about Hudson's parks and trails:**

CURRENT RESULTS	176 Total Responses		
	Agree	Neutral	Disagree
Parks within the City meet my and or my family's routine recreational needs	65% (114)	21% (37)	14% (25)
Our parks are well maintained	80% (141)	16% (29)	3% (6)
The existing trail system is adequate	23% (41)	34% (60)	43% (75)
The off-street trail system should be improved / extended	59% (104)	34% (60)	7% (12)
The existing sidewalk system is adequate	31% (55)	27% (48)	41% (73)

**Please indicate whether you agree or disagree with the following statements:**

CURRENT RESULTS	176 Total Responses		
	Agree	Neutral	Disagree
My neighborhood reflects pride of ownership and investment	82% (145)	14% (25)	3% (6)
My neighborhood is attractive	85% (149)	10% (18)	5% (9)
Transportation to/from and around my neighborhood is adequate	43% (76)	24% (42)	33% (58)
I know many of my neighbors	72% (126)	18% (32)	10% (18)

## If you could change one thing in the City of Hudson, what would it be and why?

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-  **Anonymous user's Opinion**  
Be open to new ways
-  **Anonymous user's Opinion**  
More bike trails.
-  **Anonymous user's Opinion**  
City Leaders need to help the long time residents of Hudson to embrace the diversity that comes with new blood. All need to be welcome here. People shouldn't have to live here for 20 years to be considered welcome. The brand of Hudson is tarnished by those who are afraid of change and promote hate of others.
-  **Anonymous user's Opinion**  
More thoughtful and planned expansion with attention to infrastructure needs PRIOR to development. Decisions made not on money from developers but on what is a fit for this community. And, to truly make this a welcoming community.
-  **Anonymous user's Opinion**  
I would like the City Council to be more progressive in its perspectives especially with regard to a sustainable environment, open to a more diverse population, always placing human needs above business and corporate needs, more safe cycling paths or roads, limiting the population to a self-sustaining 15,000 residents.
-  **Anonymous user's Opinion**  
I would find a way to offer an outdoor pool and water park.
-  **Anonymous user's Opinion**  
Strive to become a more accepting, inclusive community
-  **Anonymous user's Opinion**  
Add public transit.
-  **Anonymous user's Opinion**  
more transparent government.
-  **Anonymous user's Opinion**  
Low and middle income housing. Housing is tending towards the upper and extreme upper levels. It's great the Hudson is such a wonderful place to live that people want to make this their home. However, the lack of public housing or housing assistance is a huge problem for people with limited incomes. And people who are what we'd term "middle class" have difficulty finding housing within their budget. I understand developers have to charge a certain amount to cover their costs of bringing in roads and services, but we are leaving out a large population of people in finding affordable housing.
-  **Anonymous user's Opinion**  
more bike paths
-  **Anonymous user's Opinion**  
Transportation links and options...
-  **Anonymous user's Opinion**  
Get snow plows here faster so we don't get stuck in cul de sacs. Get a pool and mini golf course to force families outside. Keep family fresh. Get rid of paid meters so other towns people want to come here and spend their money on our businesses.
-  **Anonymous user's Opinion**  
To be welcoming to all regardless of ethnicity, culture.
-  **Anonymous user's Opinion**  
Turn the business area on the river and near the I94 bridge into more of a community facility.
-  **Anonymous user's Opinion**  
Affordable housing
-  **Anonymous user's Opinion**  
Law enforcement needs diversity and de-escalation training. Some officers are rude and disrespectful.
-  **Anonymous user's Opinion**  
The city should adopt an inclusion message. Where the leadership goes, the people follow. There is no reason to not welcome people of all races and religions. Use RF as an example and follow it.



**Anonymous user's Opinion**

There should be a noise ordinance. Drivers on Carmichael heading south from Hanley Rd use the boulevard to rev engines and as a drag strip. On nice days, the noise from Harleys is deafening.



**Anonymous user's Opinion**

The city council and add a parking ramp behind the fire station



**Anonymous user's Opinion**

The new ambulance group that drive around way more than Dt. Croix EMS did wasting more money!



**Anonymous user's Opinion**

Have more off the buildings be accessible to wheelchairs and strollers. Not allow sex stores.



**Anonymous user's Opinion**

Stop multi level development downtown and around river.



**Anonymous user's Opinion**

Allow all homeowners the right to choose their garbage removal vendor! YOUR vendor just increased fees by over 30%!!!!



**Anonymous user's Opinion**

As a river community, take more interest in facilitating use of the river.



**Anonymous user's Opinion**

Bike path and access, a streamline public transport into the cities for work, affordable rental housing for those just above "low income housing standards"



**Anonymous user's Opinion**

The new developments (commercial or residential) need more design criteria. We also need a board to protect the downtown historic areas from being torn down.



**Anonymous user's Opinion**

We need flashing lights for the cross walks at the middle Ann's high school. It is way too dark in the morning drop off times and I fear some child with get run over due to lack of visibility. Also keep the town quaint. It has really built up in the past 10 years, we are growing a bit too quickly.



**Anonymous user's Opinion**

Less decisions at the board level that negatively impact taxes and communities. Trying to charge for parking at the library was one such ridiculous option, another suggestion was to remove memorials? Seriously think about the people in this community and not just yourselves.



**Anonymous user's Opinion**

Public transportation



**Anonymous user's Opinion**

Sidewalks. I live near EP Rock and kids have to walk in the street. It's dangerous for walkers and drivers.



**Anonymous user's Opinion**

That they would stop allowing so many condo buildings to come into the city. I would also like to see more building improvements made to the businesses along Coulee. It's such an unattractive side to the highway. I would also like to see the boulevards maintained better- all summer long they're full of weeds - we have no trees planted along our streets or flowers.



**Anonymous user's Opinion**

School board. Out of touch egotistical self centered jerks



**Anonymous user's Opinion**

I would add public transportation options. A loop around the city of Hudson for seniors, teens, and others, as well as busing to the Twin Cities. The Hudson Loop could be: Library/beach, high school, YMCA, Target lot (and whatever replaces Family Fresh), Hudson Hospital & Clinic, Home Depot/Menards, Hockey Rink/Catalyst, Post Office, City Hall, and back to the Library. Why? Because public transportation promotes independence for senior citizens and non-drivers (including kids.) It would really make the City of Hudson a user-friendly community.



**Anonymous user's Opinion**

More downtown parking



**Anonymous user's Opinion**

Neighborhood events, such as 3rd street trick or treating should be expanded/encouraged.



**Anonymous user's Opinion**

Keep low income, section 8 housing out of family neighborhoods and keep them with the general rentals.



**Anonymous user's Opinion**

Get rid of Afton Cruise lines and use docks for visiting boaters - solves parking issue in the near term!

**Anonymous user's Opinion**

-  Increased enforcement on garbage houses, bring up to code.
-  **Anonymous user's Opinion**  
Traffic flow around 94/Carmichael; prefer not to actually see the wastewater facility as you enter via exit 1 (maybe a nice fence?)
-  **Anonymous user's Opinion**  
high taxes
-  **Anonymous user's Opinion**  
Sidewalk improvement. Areas out by old dog track to the school. Also, cleaning them of in winter.
-  **Anonymous user's Opinion**  
Re zone or re plan the city's strategic growth plan such that the rural character of Hudson is maintained. We can accept civill growth, but it must be controlled. Avoid strip malls, microp developments, storage facilities, etc.
-  **Anonymous user's Opinion**  
Larger grocer like a bigger Walmart to have greater access to more affordable groceries without the need to travel to a couple towns away to utilize more affordable prices when in a lower income household
-  **Anonymous user's Opinion**  
You cannot get around without a car. I wish there was some sort of small bussing system.
-  **Anonymous user's Opinion**  
The mayor. He seems to drive a personal agenda without consideration of public opinion, ie, EMS privatization, parking privatization, etc.
-  **Anonymous user's Opinion**  
More diverse
-  **Anonymous user's Opinion**  
Fewer new businesses and huge buildings as its starting to look like Woodbury. If someone wants that kind of population density, and a Burger King on each corner, Woodbury would be a great place to move!
-  **Anonymous user's Opinion**  
Sidewalks on all streets to keep pedestrians safe.
-  **Anonymous user's Opinion**  
Better use of the waterfront. More establishments that leverage river view. Perhaps make street that abuts the waterfront park a "pedestrian area" with limited driving similar to europe.
-  **Anonymous user's Opinion**  
Traffic flow N to S, main drag.
-  **Anonymous user's Opinion**  
Need parking!
-  **Anonymous user's Opinion**  
Side walks need to be better maintained, weeds in cracks etc.
-  **Anonymous user's Opinion**  
I think it's the growth. I understand you need taxes but seems like everything is being approved at incredible pace. The small town feel is going away. Also do more to make sure all approvals on buildings are appropriate for area. A log building in downtown does not fit the profile of old historic town.
-  **Anonymous user's Opinion**  
How about a small bus that runs a couple of times a day so that seniors can go downtown and enjoy the restaurants, shops etc.
-  **Anonymous user's Opinion**  
Off street lane for walking and biking
-  **Anonymous user's Opinion**  
Add VFW Add sidewalk system on Stageline from Heritage Greens and Red Cedar Canyon West to 11th Street bridge. And from Carmichael West on Coulee to the 11th Street bridge.
-  **Anonymous user's Opinion**  
Traffic engineering and planning. There are numerous areas that are already congested and dangerous. There are two specific areas of which I have concerns. Most pressing is the Carmichael corridor (desperately needs to be four lane with a separate bike/pedestrian lane) from I-94 to at least the middle school and preferably past the elementary. There have also been many near misses at Carmichael and Vine. Not only is there too much traffic for the signal structure, but the alignment of Carmichael through the intersection offers very reduced visibility. My other concern is Coulee Rd. I believe it could be made three lanes within the existing curbs, or more certainly within the existing right-of-way. Two lanes eastbound would allow for left turns into the businesses and side roads, without stopping traffic headed east. Only one late is needed westbound as there are no left turns required.



**Anonymous user's Opinion**

The city needs public transportation with a connection to the Twin Cities. We need young families in this community that will bring more ideas and new voices. For decades Hudson has struggled with who it wants to be - pulled by the growth of any suburban community on I94 yet wanting to remain a "small" town. I don't think Hudson has had quality community engagement about who we really are and the image we want to project. This community has been very closed off to many. If you were not born and raised in Hudson there is a vibe that you are not one of "us." A "good old boys" culture that does not communicate fairness and equity. Hudson is a very hard community to feel welcome in - even if you have lived here for years.



**Anonymous user's Opinion**

Make the city more wheelchair accessible - curb cuts need to be improved, safe pathways to shopping areas on the hill need to be created, sidewalks need to be connected (too many "deadend" walks). Redo current paths that do not meet ADA requirements (too steep). People with limited mobility have the same rights as everyone else to move around the city safely.



**Anonymous user's Opinion**

What's the status of the sidewalk inventory? I've scraped the leg hangers on two wheelchairs on the downtown curb cuts. There are probably non-standard or non-compliant access points sprinkled throughout the city.



**Anonymous user's Opinion**

New development seems to be ugly... downtown and on the hill. Huge ugly apartment buildings at the corner of Carmichael and Vine are out of place with Hudson's small town charm. Development is happening before roads are fixed and will only lead to more Traffic gridlock. It's backwards and will create more problems that can't be well remedied. The roads should have been planned first, and then development that the infrastructure can support could be approved.



**Anonymous user's Opinion**

Stronger connection to community and school district. The school district feels like a separate entity of the community. Would love to see a community center, community pool or youth center that isn't tied to the YMCA



**Anonymous user's Opinion**

repave roads on a regular basis, not just filling holes with that rock spray



**Anonymous user's Opinion**

I would personally love another Kwik Trip. I think we need to fix the Carmichael/Crestview/94 intersections. The number of people running the light and turning into the wrong lights is just accidents almost happening all the time.



**Anonymous user's Opinion**

Vacancy of commercial buildings should not be allowed. If the owner cannot sell the building within a year they should be taxed heavily enough to encourage the building to be torn down and have green space put in at a very low tax rate.



**Anonymous user's Opinion**

Facilities/shelters for the homeless, & services to support our veterans.



**Anonymous user's Opinion**

Less emphasis on un planned, unattractive building growth. Economic growth does not always come from another motel chain. The city needs a well thought out development plan that is not "bent" every time a developer asks for a variance



**Anonymous user's Opinion**

Consistent sidewalks, especially connecting different neighborhoods and parks.



**Anonymous user's Opinion**

More sidewalks. It's a relatively small town and it should be easier to get around safely without a car.



**Anonymous user's Opinion**

A small parking ramp back by the fire dept would be nice . Something set back off of 2nd street. NOT some huge building right on 2nd street.



**Anonymous user's Opinion**

Add public transportation. Around Hudson and to the St. Paul Depot and Mall of America.



**Anonymous user's Opinion**

Inner section of Carmichael and Vine



**Anonymous user's Opinion**

Better access to public and local government discussions and decisions with an impact on community



**Anonymous user's Opinion**

Better newspaper coverage of events.



**Anonymous user's Opinion**

See more diversity and an inclusive welcoming spirit. We can present as cold clannish and not as all inclusive.



**Anonymous user's Opinion**

The unintended consequence of the new parking meter implementation will be a decrease of economic opportunity for Hudson. Boaters often spend many hours on their boats and overnights. A problem arises if boaters and guests that arrive by car and have no long term parking available. If all the new parking meters in Hudson have a 2 hour limit it restricts boater and guest opportunities.



**Anonymous user's Opinion**

Better opportunities for fishing on the St. Croix River. I bought a season pass to the boat launch and it was closed due to high water much of the year. Also, the large boats on the river can make it dangerous for smaller fishing boats as they drive too fast, throw out too large a wake, and go way too close to fishing boats.



**Anonymous user's Opinion**

more sidewalks



**Anonymous user's Opinion**

Nothing.



**Anonymous user's Opinion**

I wish Hudson had a more diverse population. I want my children to grow up around people of all faiths, thoughts, colors, etc. It helps to create a greater sense of the world and enables better adaptability.



**Anonymous user's Opinion**

There is too much new development to support the type of community that is Hudson



**Anonymous user's Opinion**

Less urban sprawl. What can be done to keep hudson unique?



**Anonymous user's Opinion**

Sidewalks on Carmichael road to Vine Street. And whoever else sidewalks are missing or incomplete.



**Anonymous user's Opinion**

Hudson is known for being clique-ish and people being hard to get to know. We've traveled a lot, lived all over - lived here 14 years - and found this to be true. We only started to feel like we belonged here maybe 2 years ago... It's very pricey



**Anonymous user's Opinion**

We need a community center/pool.



**Anonymous user's Opinion**

Like to see a full time FD that doubles as EMS.



**Anonymous user's Opinion**

I'd like the city to become a leader in renewable energy use and sustainable practices.



**Anonymous user's Opinion**

Some city streets need to be improved & widened to accommodate the growing city and new construction and expansion areas.



**Anonymous user's Opinion**

Slow down growth and improve infrastructure first to manage traffic in a safe manner



**Anonymous user's Opinion**

Enough expansion. The city is too busy to adequately enjoy. Everything converges into a small area and there are too many for too little. No expansion on "affordable housing." A worsening social element has already been increasing as a result.



**Anonymous user's Opinion**

Repair, maintain & cleanliness of downtown sidewalks. Perception and safety issues for residents and prospective residents and tourists needs to be a priority.



**Anonymous user's Opinion**

There would be more help with newbies with eyesores and junk strewn around they're property. 25 years waiting for the city to do something.



**Anonymous user's Opinion**

Build a parking ramp downtown.



**Anonymous user's Opinion**

Would be nice not to have redetermined service providers for refuse, internet, cable and phone. Also lack of public recycling facilities and/or landfills.



**Anonymous user's Opinion**

A city bus system and/or a bus that connects to MN



**Anonymous user's Opinion**

More sidewalks on Carmichael built toward Vine



**Anonymous user's Opinion**

Add a Hy-Vee or Trader Joe's (or both) to compete with overpriced and crowded County Market.

-  **Anonymous user's Opinion**  
public transportation
-  **Anonymous user's Opinion**  
I wish I would have known how cliquy this town was before I moved here. It is hard to get to know people if you are viewed as an 'outsider'.
-  **Anonymous user's Opinion**  
Would prefer a slow in growth.
-  **Anonymous user's Opinion**  
Figure out a way to get rid of the Royal Inn Motel. It is an eyesore and a dump.
-  **Anonymous user's Opinion**  
PARKING availability!!!!!! And get rid of the facade of the Smiling Moose, it does not fit the rest of the downtown area at all and looks ridiculous.
-  **Anonymous user's Opinion**  
The new parking meters, not sure that this system was necessary, doesn't seem to be as convenient as the individual meters and it took away from the small town feel. Traffic pattern at Vine Carmichael needs to be addressed, it is congested and dangerous as well is the Carmichael exit off 94.
-  **Anonymous user's Opinion**  
Free parking everywhere in the city of Hudson. Especially the business district downtown. We want to encourage people to frequent the downtown businesses. Why not build a free parking ramp to make it easier 4 people 2 support the downtown business district? A logical place would be the parking lot to the north of the Wells Fargo Bank building . Also, now that the fire station is moving to the area east of Plaza 94 , why not demolish the old station and put a free parking ramp there? Or, you could convert the parking lot that is directly north of City Hall into a free parking ramp. If River Falls and New Richmond manage to provide free parking, why not Hudson?
-  **Anonymous user's Opinion**  
I would want to see greater care for and connection with neighbors
-  **Anonymous user's Opinion**  
Stop the development.
-  **Anonymous user's Opinion**  
We need a newspaper or other media to keep us informed. Rivertown does not cut it.
-  **Anonymous user's Opinion**  
On street leaf pick up - obvious reasons. At least a compost facility!
-  **Anonymous user's Opinion**  
More parking in the downtown area if you want people to go there to shop and dine. Free please.
-  **Anonymous user's Opinion**  
The Mayor. We need a new one.
-  **Anonymous user's Opinion**  
More transportation for people of all abilities. There is inadequate transportation for lower income and people with disabilities. Especially to NR and RF.
-  **Anonymous user's Opinion**  
Our town needs to be more inclusive of all people. There is a lot of ugly, fearful, hate laced behavior and words ion Hudson that make me embarrassed for our town. I was incredibly disappointed and ashamed that our town leaders refused to stand up to hate. This refusal by our public leaders is seen as condoning hateful behavior.
-  **Anonymous user's Opinion**  
There is too much development we do not need it. Our community will suffer from it.
-  **Anonymous user's Opinion**  
Need more police officers / police supervisors
-  **Anonymous user's Opinion**  
That things look outdated, buildings sometimes look empty or unsightly, things unkept. Many businesses off of 94 are unappealing.
-  **Anonymous user's Opinion**  
I feel a hostile and unfriendly culture. Not open and welcoming.
-  **Anonymous user's Opinion**  
I would like to see more done to protect the river, including permeable pavement and more rain gardens.



**Anonymous user's Opinion**

I wish the city would think about traffic flow over I94. Having just 2 bridges makes driving in Hudson a problem.



**Anonymous user's Opinion**

More round-a-bouts to ease traffic and slow fast sections. Reduction in high speed driving in Hudson



**Anonymous user's Opinion**

I would support some kind of public transportation for those who don't drive or own automobiles and to cut down on auto emissions.



**Anonymous user's Opinion**

additional parking, increased size of individual parking spaces. extremely difficult to visit downtown establishments. strip mall parking spaces are cramped and inadequate.



**Anonymous user's Opinion**

More grants for individual and community Development in the Parks because parks are underutilized and provide very little more open forms that we met two different political views . No interest interest except for the lake and those who like to fish otherwise it's pretty Barron regarding cultural awareness



**Anonymous user's Opinion**

More attractive and diverse development on the hill, it's too generic /cookie cutter it should reflect /echo the charm of downtown



**Anonymous user's Opinion**

We are one of the most highly taxed counties in Wisconsin. While the city government is generally effective, we need greater focus on efficient use of tax dollars with an emphasis on providing fundamental services (streets, police, fire dept) . We should not have to resort to nickle-and-dime initiatives such as downtown parking meters to make ends meet.



**Anonymous user's Opinion**

I would go back in time. I've lived here since 1991 and I don't think the town is getting better to live in with all the new houses and fast food restaurants and no good paying jobs.



**Anonymous user's Opinion**

Walk ability



**Anonymous user's Opinion**

limit growth or restrict area for typical suburban business/sprawl



**Anonymous user's Opinion**

Strong plan to aggressively grow available affordable housing to east and south.



**Anonymous user's Opinion**

Make public docks along the dike and con sessions and venders on the dike. Promoting safe and fun summer activities by the river.



**Anonymous user's Opinion**

A Parks and Recreation Department that takes on creating more centralized infrastructure where community sponsored activities and sports can be take place.



**Anonymous user's Opinion**

More racial diversity



**Anonymous user's Opinion**

Modernize and upgrade the parks along the river!



**Anonymous user's Opinion**

I would add more, not change. Encourage the developments to communicate with the city and vice versa. Create a platform easy to use for all citizens to be able to find out what is going on and how to get more involved



**Anonymous user's Opinion**

I would have a more attractive facade along 1st St and the River. Phipps is great, Library great, Pier great, the rest of it - is atrocious. Shame on everyone who allowed that Valley Cartage monstrosity. That is EXACTLY what no one wants - blocking the other building across the street - blocking a view from any pedestrian - shame shame shame. It's awful. Making the buildings, businesses, open areas across from Lakefront be a fabulous, beautiful area is what I would change.



**Anonymous user's Opinion**

Stop Building, leave it as it is, it's gotten so big that housing costs are unaffordable, miss the tiny town of 30 years ago



**Anonymous user's Opinion**

Public tennis courts. This is the only city I know of without a bank of public tennis courts to use. The high school has courts but they belong to the district and are often booked with school events and lessons. You cannot count in availability.



**Anonymous user's Opinion**

move the water treatment plant away from the entrance to downtown. It's a disgusting sight and smell when entering Hudson. It detracts from the entire area.



**Anonymous user's Opinion**

I would change the development on Carmichael and Vine. Unnecessary.



**Anonymous user's Opinion**

Address the lack of parking in the downtown area. I'm to the point of avoiding downtown because parking is hard to find, then I have to mess around with the meters. AND, Vine Street from High School to Carmichael needs a full sidewalk. Traffic is congested during school start and end times, so Vine St. was just improved, but something should have been done to improve traffic flow during these times - same with Carmichael by the Middle school. These 2 roads are a nightmare during start and end times.



**Anonymous user's Opinion**

More pre-planning and traffic management.



**Anonymous user's Opinion**

Increased diversity



**Anonymous user's Opinion**

Fix the intersection of Carmichael and freeway by County Market and ramp exits. It is a nut house.



**Anonymous user's Opinion**

Change parking enforcement back to 9-5pm and free on weekends. Not sure why Hudson wants to make it laborious to come to Hudson to shop, eat and take Phipps classes. It has been painful to watch people struggle with the new parking meters, and I think its a shame that not all of the meters tell people parking is free from Thanksgiving to Christmas. That should be something to advertise - a small gift goes a long way to endear people to this lovely town.



**Anonymous user's Opinion**

I think that all development in Hudson should have sidewalks. (not pathways) Sidewalks are a great walking asset and they help you to meet neighbors and give a safe way getting out and about. People come to old town to walk their dogs on the sidewalks. Kids need a safe way to get to friends houses.



**Anonymous user's Opinion**

No walking or biking on Trout Brooke. There are more than 13 completely blind turns on that road. People walk on the road because there is no sidewalk and they don't want to walk in the grass. The road is wide enough for barely 2 cars as is. When there are people walking and/or riding bikes, they are creating a hazardous situation.



**Anonymous user's Opinion**

To reconstruct existing roadways to stay ahead of development. Recent projects seem to be pavement replacement projects and not improvement projects. Make investments that align with future development, improve existing intersections with more than just additional turn lanes. Include pedestrian and bike facility improvements with every roadway project.



**Anonymous user's Opinion**

I would love to see more diversity (race/ethnicity) in our community.



**Anonymous user's Opinion**

more public transportation. I hate that I have to drive my car anywhere I need to go. I'd like to be able to take a bus



**Anonymous user's Opinion**

Lower the property taxes. They are extremely high.



**Anonymous user's Opinion**

Be more top of mind with day or weekend tourism



**Anonymous user's Opinion**

sidewalks and bike accessibility. It is very difficult to safely walk or bike in this community. While there are pockets they are not connected.

## What is the one thing about Hudson that should not change and why?

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**Anonymous user's Opinion**

Do not lose the value of Lakefront Park



**Anonymous user's Opinion**

Downtown connection to the river. It's a centerpiece that needs to be supported and maintained.



**Anonymous user's Opinion**

Lakefront Park and Second Street.



**Anonymous user's Opinion**

The local, downtown atmosphere is very good; no more outside businesses and restaurants should be allowed to build large patios or high buildings to block others' views.



**Anonymous user's Opinion**

The pride in the condition of the community and the vibrancy of the downtown. Rents are driving businesses away (I am not a business owner).



**Anonymous user's Opinion**

Maintain and improve public facilities and access at the St. Croix River waterfront, because that is Hudson's unique beneficial character for both residents and visitors.



**Anonymous user's Opinion**

The small-town, "Norman Rockwell" feel. Low cost parking on 1st & 2nd Streets



**Anonymous user's Opinion**

Hudson seems to be pretty good at offering a wide range of interesting things for people to participate in: library, the phipps, restaurants, festivals, learning experiences, outdoor activities at the state park, chamber sponsored events.



**Anonymous user's Opinion**

Hot Air Affair - super fun event! Community builder!



**Anonymous user's Opinion**

It's small-town feel.



**Anonymous user's Opinion**

Building more houses. We don't need more people here to overpopulate our one middle and one high school.



**Anonymous user's Opinion**

Our beautiful downtown and lakefront !



**Anonymous user's Opinion**

Keep the box type and franchise businesses on the hill and the small town community feel downtown.



**Anonymous user's Opinion**

Downtown shopping keep it local



**Anonymous user's Opinion**

The small town feel. Keep chain businesses out of downtown. Love the addition of the Trolley



**Anonymous user's Opinion**

Maintain historic sites, especially downtown



**Anonymous user's Opinion**

The older historic buildings on second street should stay!



**Anonymous user's Opinion**

The look and character of down town



**Anonymous user's Opinion**

The small town And old time feel of the downtown. I don't want it to ge more developed.

-  **Anonymous user's Opinion**  
Do not lower parks and park maintenance budget
-  **Anonymous user's Opinion**  
Main Street / downtown area, keep the small town feel to that area.
-  **Anonymous user's Opinion**  
Maintain the historic character; too much emphasis recently on redevelopment with commercial and multifamily residential that is WAY out of character.
-  **Anonymous user's Opinion**  
Downtown "small town" feel
-  **Anonymous user's Opinion**  
Keeping the downtown area as is.
-  **Anonymous user's Opinion**  
Please keep Hudson small. It has a great feel about it! There are many homes and businesses currently. I feel we do not need to continue to expand. I would like to see us stay somewhat separated from the urban nature that is feeding into Hudson.
-  **Anonymous user's Opinion**  
Downtown
-  **Anonymous user's Opinion**  
Pride in the river. I love how we showcase the natural beauty.
-  **Anonymous user's Opinion**  
The downtown area should ALWAYS keep building requirements to reflect the historical nature of the city. Future multi family housing that must pass city council should also have requirements that encourage a "classic" appearance. The Modern look of current buildings will quickly go out of style. Hudson is a historic river town & the buildings should reflect that
-  **Anonymous user's Opinion**  
It's small-town charm.
-  **Anonymous user's Opinion**  
Willow River Elementary School should remain an active elementary school.
-  **Anonymous user's Opinion**  
The small town atmosphere.
-  **Anonymous user's Opinion**  
No multi-storied buildings in the downtown area - only allow existing heights.
-  **Anonymous user's Opinion**  
Historic feeling of the down town area.
-  **Anonymous user's Opinion**  
family community
-  **Anonymous user's Opinion**  
Preserve the small town and community knit culture. We are a small town and we should strive to NOT become yet another suburb of the twin cities.
-  **Anonymous user's Opinion**  
Unsure
-  **Anonymous user's Opinion**  
That we are a community that takes pride in the arts. Its sad to see at the high school level how little is put into the arts in comparison to athletics.
-  **Anonymous user's Opinion**  
The library. It is a landmark for the city. It is something to be proud of.
-  **Anonymous user's Opinion**  
Maintain the small town look and feel- don't become a Stillwater. Stillwater is chaotic and has lost its charm, its chaotic and touristy.
-  **Anonymous user's Opinion**  
Small tow feeling. Enough with the repeated businesses like cash store, vape store, etc.

-  **Anonymous user's Opinion**  
Park space and free space should be maintained or increased where possible. Avoid the temptation to over develop.
-  **Anonymous user's Opinion**  
No changes needed for trails and bikes. The taxes cannot go up. We cannot tax people out.
-  **Anonymous user's Opinion**  
Vibrant downtown. Makes it feel less suburban
-  **Anonymous user's Opinion**  
Keep size at what it is.
-  **Anonymous user's Opinion**  
Everything is subject to change at some point
-  **Anonymous user's Opinion**  
That there is not a parking structure on Second Street. I agree with the parking necessity, but disagree with adding additional congestion to Second Street. I think the HFD house would be an ideal option. City owns it. Revenue bearing. Surrounding residents are familiar with having a non-residential neighbors.
-  **Anonymous user's Opinion**  
Sense of community - but, tempered with hubris.
-  **Anonymous user's Opinion**  
Lakefront Park and the communities support of the arts.
-  **Anonymous user's Opinion**  
1. Lakefront Park is a real gem with river access, great site lines, wheelchair access and space to walk or jog. Don't let commercial developers mess it up with priority of profits more important than its use as a civic or community asset. DO NOT take the bait of "jobs" as a trade for all the park represents. 2. Make every effort to cooperate with nearby communities, townships and counties to keep Concentrated Area Feeding Operations (CAFOs) far, far away. I mean FAR away -- hundreds of miles away -- like in Iowa, southern Minnesota or southern Illinois.
-  **Anonymous user's Opinion**  
Try to keep the small town charm with new development. I don't want Hudson to look like Woodbury. Plan infrastructure before commercial and residential development goes in.
-  **Anonymous user's Opinion**  
the city and businesses need to get the walkways plowed within 24 hours as homeowners are required to do
-  **Anonymous user's Opinion**  
Remember that we are residents of WI and not MN. That we can maintain that sense of small community while still growing. Remember to be kind to each other and as a community.
-  **Anonymous user's Opinion**  
The old town charm in the downtown district.
-  **Anonymous user's Opinion**  
Maintain the family-friendly, small town atmosphere, the main reason we moved here in the first place.
-  **Anonymous user's Opinion**  
Small town atmosphere. with a nice downtown and free parking
-  **Anonymous user's Opinion**  
small-town feel.
-  **Anonymous user's Opinion**  
Downtown shopping and restaurant are brings in a lot of money from outside visitors - we are so lucky to have that.
-  **Anonymous user's Opinion**  
The small town feel of downtown. Living downtown we absolutely love walking downtown for shopping, dinner, entertainment. It's wonderful! Love all the independent businesses and have gotten to know so many of the wonderful business owners.
-  **Anonymous user's Opinion**  
Arts focus & Hot Air Affair
-  **Anonymous user's Opinion**  
Downtown
- Anonymous user's Opinion**



Small town feel.



**Anonymous user's Opinion**

Our focus on our unique setting and natural resources.



**Anonymous user's Opinion**

Tourist friendly, brings in quality people.



**Anonymous user's Opinion**

Orientation to and protection of the St. Croix River front.



**Anonymous user's Opinion**

Nothing.



**Anonymous user's Opinion**

Hudson is a small community with pleasant open spaces. It should stay this way.



**Anonymous user's Opinion**

Riverfront



**Anonymous user's Opinion**

Not to become too commercialized and lose the small-town feel and intimate community.



**Anonymous user's Opinion**

It's beautiful here



**Anonymous user's Opinion**

The small town tight knit community. If we expand too much this will not exist anymore.



**Anonymous user's Opinion**

Keep allowing restaurants and healthy competition through the city. Downtown and up.



**Anonymous user's Opinion**

Old downtown main street and St. Croix river park.



**Anonymous user's Opinion**

The small town feel



**Anonymous user's Opinion**

Small town feeling most often. Local, not expansive community.



**Anonymous user's Opinion**

Downtown local businesses and restaurants ability to continue operating with local consumer support.



**Anonymous user's Opinion**

The downtown area should remain quaint. Not overrun with unattractive buildings.



**Anonymous user's Opinion**

Keep the maintenance of public areas clean as they have been..



**Anonymous user's Opinion**

Historic downtown.



**Anonymous user's Opinion**

The downtown needs to remain local businesses!!!



**Anonymous user's Opinion**

Please continue concerts and events near the bandshell downtown.



**Anonymous user's Opinion**

Sense of community and community events



**Anonymous user's Opinion**

I love our thriving downtown. No empty storefronts, lots of shopping and eating options.

-  **Anonymous user's Opinion**  
Open space, not urban feeling
-  **Anonymous user's Opinion**  
The old town look and feel of downtown, it already shows signs of that disappearing and their has to be a happy medium between growth and development standards. The strip mall across from Ziggy's seems to have found that medium.
-  **Anonymous user's Opinion**  
Maintain a size roughly of where we are at now. Residents genuinely appreciate the charm and advantages that come with our current size. Not interested in grow and develop, grow and develop and all the problems that comes with chasing growth.
-  **Anonymous user's Opinion**  
The variety of shopping available, only add to it with some major stores.
-  **Anonymous user's Opinion**  
No public transportation is needed. Keep the small town feel, Hudson is a city where one can easily walk, enjoy the views and feel safe.
-  **Anonymous user's Opinion**  
That it should be an affordable, clean, safe place for current citizens. Keep the city the beautiful , small town that it is by not making any big expansions that are simply going to drive up our taxes and the cost of city services. We are a small river town with a community of people who really care about each other , care about the small local businesses, and care about the quality of our town. Keep it that way.
-  **Anonymous user's Opinion**  
It's identity as a town and not a suburb, even though we enjoy many suburban advantages
-  **Anonymous user's Opinion**  
Stop all sustainability agenda. It is too expensive and used mostly by the most vocal.
-  **Anonymous user's Opinion**  
Focus on the St. Croix River.
-  **Anonymous user's Opinion**  
????
-  **Anonymous user's Opinion**  
I really like the riverfront area and the free access the public has to it.
-  **Anonymous user's Opinion**  
Our downtown has developed into a fun place to shop and eat. Even more variety in food options would be great! \*\*Now, it is time to work on that parking garage!
-  **Anonymous user's Opinion**  
Home/small town feel
-  **Anonymous user's Opinion**  
Great sense of community
-  **Anonymous user's Opinion**  
Willow River Park. Love that place.
-  **Anonymous user's Opinion**  
It's focal point of the river and it's positive support of cultural events.
-  **Anonymous user's Opinion**  
The multitude of locally owned businesses, especially downtown. I do not want to see chains downtown.
-  **Anonymous user's Opinion**  
The focus on the river. That is a very attractive asset to the city.
-  **Anonymous user's Opinion**  
Keep our downtown Locally Owned. We need this to ensure that we have a great mix of dining and shopping that is unique.
-  **Anonymous user's Opinion**  
The "small town" feeling



access to and maintenance of river front. this is what encourages visitors and residents to come down town.



**Anonymous user's Opinion**

Make the Main Street attractive and not overcrowded and not to attract tourists but people really like to be there for the Lively array and variety of shops.



**Anonymous user's Opinion**

Downtown decorations, adds to city charm.



**Anonymous user's Opinion**

Keep the downtown for the people and don't keep selling off land to build more condos.



**Anonymous user's Opinion**

Small town charm



**Anonymous user's Opinion**

unique small town feel



**Anonymous user's Opinion**

Do not overdevelop downtown.



**Anonymous user's Opinion**

Don't stop asking for opinions of the people who live here.



**Anonymous user's Opinion**

Small town feel



**Anonymous user's Opinion**

The quaint downtown look, feel, access to shops. Please don't let this become another Woodbury or Stillwater - don't let downtown become a bunch of new updated multi level buildings. It's loved because of what it is - the age, the character, the history, the charm - you can't go back once you allow these changes.



**Anonymous user's Opinion**

do not add more fast food restaurants! I don't believe the population can support the survival of so many restaurants.



**Anonymous user's Opinion**

The charming downtown with its unique restaurants and shopping.



**Anonymous user's Opinion**

Wide open spaces— felling like you're away from the city.



**Anonymous user's Opinion**

The down town stores are nice and attract people to our town. (But parking...)



**Anonymous user's Opinion**

The amount of green-space should not be reduced. Hudson has very nice parks.



**Anonymous user's Opinion**

The small town atmosphere. There are too many people moving in which means more traffic, more restaurants, extra inconveniences like parking issues,



**Anonymous user's Opinion**

Concerts in the park, Light Up Night, Hot Air Affair and all the other community events Hudson hosts.



**Anonymous user's Opinion**

Do not change the hometown flavor. Our downtown has developed to a fun place to come, enjoy and then leave. Good for commerce. Do NOT add living quarters to downtown and develop it with the old town architecture.



**Anonymous user's Opinion**

Bandshell & lakefront area



**Anonymous user's Opinion**

Preserve the downtown area and riverfront.



**Anonymous user's Opinion**

We love the community feeling and hope this continues well into the future.



**Anonymous user's Opinion**

It's downtown, it's cute and accessible



**Anonymous user's Opinion**

Hudson has a unique topography with the historic downtown area, and "the Hill". I like that and both should be promoted for the positive attributes they each possess.



**Anonymous user's Opinion**

Its support of a unique downtown, local businesses, retail and restaurants. It has a vibrant cultural scene that greatly adds to its appeal

## In what municipality do you reside?

CURRENT RESULTS		176 Total Responses	
<b>A</b>	City of Hudson (105)	<div style="width: 60%;"></div>	60% (105)
<b>B</b>	Village of North Hudson (14)	<div style="width: 8%;"></div>	8% (14)
<b>C</b>	Town of Hudson (40)	<div style="width: 23%;"></div>	23% (40)
<b>D</b>	Town of Troy (7)	<div style="width: 4%;"></div>	4% (7)
<b>E</b>	Town of St. Joseph (7)	<div style="width: 4%;"></div>	4% (7)
<b>F</b>	Other: (please identify in next question) (3)	<div style="width: 2%;"></div>	2% (3)

REGISTERED VS NON-REGISTERED	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
Registered Voters (106)	59.4% (63)	9.4% (10)	23.6% (25)	3.8% (4)	3.8% (4)	-
Non-Registered Voters (70)	60.0% (42)	5.7% (4)	21.4% (15)	4.3% (3)	4.3% (3)	4.3% (3)

ALL RESPONDENTS	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
All respondents (176)	60.0% (105)	8.0% (14)	23.0% (40)	4.0% (7)	4.0% (7)	2.0% (3)
Registered Voters in Hudson, WI, WI (106)	59.4% (63)	9.4% (10)	23.6% (25)	3.8% (4)	3.8% (4)	-
Live in Hudson, WI, WI (168) - Self-reported	61.3% (103)	7.7% (13)	23.2% (39)	4.2% (7)	3.6% (6)	-
Subscribers to Hudson, WI, WI (168)	61.3% (103)	7.7% (13)	23.2% (39)	4.2% (7)	3.6% (6)	-
Register respondents from anywhere (111)	58.0% (64)	9.0% (10)	23.0% (25)	4.0% (4)	5.0% (5)	3.0% (3)

AGE RANGE	111 REGISTERED VOTERS					
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
18-29 (1)	-	-	100.0% (1)	-	-	-
30-39 (9)	77.8% (7)	11.1% (1)	11.1% (1)	-	-	-
40-49 (14)	57.1% (8)	7.1% (1)	21.4% (3)	7.1% (1)	-	7.1% (1)
50-59 (21)	47.6% (10)	9.5% (2)	28.6% (6)	-	14.3% (3)	-
60-69 (29)	58.6% (17)	10.3% (3)	17.2% (5)	6.9% (2)	6.9% (2)	-
70-79 (18)	55.6% (10)	5.6% (1)	27.8% (5)	-	-	11.1% (2)
80-89 (3)	66.7% (2)	-	33.3% (1)	-	-	-
unknown (16)	62.5% (10)	12.5% (2)	18.8% (3)	6.3% (1)	-	-

CITY WARD	111 REGISTERED VOTERS					
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
Unknown (111)	57.7% (64)	9.0% (10)	22.5% (25)	3.6% (4)	4.5% (5)	2.7% (3)

VOTERS GENDER	111 REGISTERED VOTERS					
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
F (66)	48.5% (32)	12.1% (8)	28.8% (19)	1.5% (1)	6.1% (4)	3.0% (2)
M (44)	70.5% (31)	4.5% (2)	13.6% (6)	6.8% (3)	2.3% (1)	2.3% (1)
Unknown (1)	100.0% (1)	-	-	-	-	-

**If you responded "Other" in question 16, please identify the municipality you live in.**

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**Anonymous user's Opinion**

Our Town of Hudson residence is just over the boundary with the City of Hudson.



**Anonymous user's Opinion**

Houlton



**Anonymous user's Opinion**

Also own home in Hudson.



**Anonymous user's Opinion**

NA



**Anonymous user's Opinion**

V



**Anonymous user's Opinion**

River Falls

**Please check all that apply:**

CURRENT RESULTS		169 Total Responses	
<b>A</b>	Live in Hudson (156)		92% (156)
<b>B</b>	Work in Hudson (49)		29% (49)
<b>C</b>	Own a business in Hudson (23)		14% (23)
<b>D</b>	Have child(ren) in / Attend Hudson School District (56)		33% (56)
<b>E</b>	Volunteer for the City (23)		14% (23)

REGISTERED VS NON-REGISTERED					
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>
Registered Voters (101)	93.1% (94)	23.8% (24)	11.9% (12)	31.7% (32)	10.9% (11)
Non-Registered Voters (68)	91.2% (62)	36.8% (25)	16.2% (11)	35.3% (24)	17.6% (12)

ALL RESPONDENTS					
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>
All respondents (169)	92.0% (156)	29.0% (49)	14.0% (23)	33.0% (56)	14.0% (23)
Registered Voters in Hudson, WI, WI (101)	93.1% (94)	23.8% (24)	11.9% (12)	31.7% (32)	10.9% (11)
Live in Hudson, WI, WI (163) - Self-reported	92.6% (151)	28.8% (47)	13.5% (22)	33.1% (54)	12.9% (21)
Subscribers to Hudson, WI, WI (163)	92.6% (151)	28.8% (47)	13.5% (22)	33.1% (54)	12.9% (21)
Register respondents from anywhere (104)	92.0% (96)	24.0% (25)	13.0% (13)	31.0% (32)	12.0% (12)

AGE RANGE						178 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	
18-29 (1)	100.0% (1)	-	-	-	-	
30-39 (9)	100.0% (9)	22.2% (2)	-	33.3% (3)	-	
40-49 (12)	100.0% (12)	16.7% (2)	8.3% (1)	58.3% (7)	8.3% (1)	
50-59 (20)	90.0% (18)	35.0% (7)	30.0% (6)	35.0% (7)	10.0% (2)	
60-69 (27)	96.3% (26)	25.9% (7)	14.8% (4)	25.9% (7)	7.4% (2)	
70-79 (16)	75.0% (12)	-	-	12.5% (2)	25.0% (4)	
80-89 (3)	100.0% (3)	33.3% (1)	-	-	-	
unknown (16)	93.8% (15)	37.5% (6)	12.5% (2)	37.5% (6)	18.8% (3)	

CITY WARD						178 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	
Unknown (104)	92.3% (96)	24.0% (25)	12.5% (13)	30.8% (32)	11.5% (12)	

VOTERS GENDER						178 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	
F (62)	91.9% (57)	27.4% (17)	9.7% (6)	30.6% (19)	9.7% (6)	
M (41)	92.7% (38)	19.5% (8)	17.1% (7)	31.7% (13)	12.2% (5)	
Unknown (1)	100.0% (1)	-	-	-	100.0% (1)	

## How long have you lived in Hudson?



**REGISTERED VS NON-REGISTERED**

	A	B	C	D	E	F	G
Registered Voters (106)	-	17.0% (18)	10.4% (11)	24.5% (26)	32.1% (34)	13.2% (14)	2.8% (3)
Non-Registered Voters (69)	5.8% (4)	24.6% (17)	5.8% (4)	26.1% (18)	27.5% (19)	7.2% (5)	2.9% (2)

**ALL RESPONDENTS**

	A	B	C	D	E	F	G
All respondents (175)	2.0% (4)	20.0% (35)	9.0% (15)	25.0% (44)	30.0% (53)	11.0% (19)	3.0% (5)
Registered Voters in Hudson, WI, WI (106)	-	17.0% (18)	10.4% (11)	24.5% (26)	32.1% (34)	13.2% (14)	2.8% (3)
Live in Hudson, WI, WI (167) - Self-reported	2.4% (4)	21.0% (35)	8.4% (14)	25.1% (42)	29.9% (50)	11.4% (19)	1.8% (3)
Subscribers to Hudson, WI, WI (167)	2.4% (4)	21.0% (35)	8.4% (14)	25.1% (42)	29.9% (50)	11.4% (19)	1.8% (3)
Register respondents from anywhere (111)	-	16.0% (18)	11.0% (12)	24.0% (27)	32.0% (35)	13.0% (14)	5.0% (5)

**AGE RANGE** 111 REGISTERED VOTERS

	A	B	C	D	E	F	G
18-29 (1)	-	100.0% (1)	-	-	-	-	-
30-39 (9)	-	22.2% (2)	33.3% (3)	33.3% (3)	-	11.1% (1)	-
40-49 (14)	-	14.3% (2)	14.3% (2)	28.6% (4)	35.7% (5)	7.1% (1)	-
50-59 (21)	-	-	-	61.9% (13)	28.6% (6)	9.5% (2)	-
60-69 (29)	-	24.1% (7)	10.3% (3)	6.9% (2)	41.4% (12)	13.8% (4)	3.4% (1)
70-79 (18)	-	-	5.6% (1)	11.1% (2)	38.9% (7)	27.8% (5)	16.7% (3)
80-89 (3)	-	-	33.3% (1)	-	33.3% (1)	33.3% (1)	-
unknown (16)	-	37.5% (6)	12.5% (2)	18.8% (3)	25.0% (4)	-	6.3% (1)

**CITY WARD** 111 REGISTERED VOTERS

	A	B	C	D	E	F	G
Unknown (111)	-	16.2% (18)	10.8% (12)	24.3% (27)	31.5% (35)	12.6% (14)	4.5% (5)

**VOTERS GENDER** 111 REGISTERED VOTERS

	A	B	C	D	E	F	G
F (66)	-	16.7% (11)	12.1% (8)	22.7% (15)	27.3% (18)	16.7% (11)	4.5% (3)
M (44)	-	13.6% (6)	9.1% (4)	27.3% (12)	38.6% (17)	6.8% (3)	4.5% (2)
Unknown (1)	-	100.0% (1)	-	-	-	-	-

## Housing: Which type best applies to you?

CURRENT RESULTS		175 Total Responses	
<b>A</b>	Detached Single Family Home (147)		84% (147)
<b>B</b>	Duplex / Two Family Home (8)		5% (8)
<b>C</b>	Apartment Complex (3)		2% (3)
<b>D</b>	Condominium (14)		8% (14)
<b>E</b>	Mobile Home (1)		1% (1)
<b>F</b>	Senior Housing (2)		1% (2)

REGISTERED VS NON-REGISTERED						
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
Registered Voters (105)	81.0% (85)	5.7% (6)	1.0% (1)	10.5% (11)	1.0% (1)	1.0% (1)
Non-Registered Voters (70)	88.6% (62)	2.9% (2)	2.9% (2)	4.3% (3)	-	1.4% (1)

ALL RESPONDENTS						
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>
All respondents (175)	84.0% (147)	5.0% (8)	2.0% (3)	8.0% (14)	1.0% (1)	1.0% (2)
Registered Voters in Hudson, WI, WI (105)	81.0% (85)	5.7% (6)	1.0% (1)	10.5% (11)	1.0% (1)	1.0% (1)
Live in Hudson, WI, WI (167) - Self-reported	83.8% (140)	4.8% (8)	1.8% (3)	7.8% (13)	0.6% (1)	1.2% (2)
Subscribers to Hudson, WI, WI (167)	83.8% (140)	4.8% (8)	1.8% (3)	7.8% (13)	0.6% (1)	1.2% (2)
Register respondents from anywhere (110)	81.0% (89)	5.0% (6)	1.0% (1)	11.0% (12)	1.0% (1)	1.0% (1)

AGE RANGE							110 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	
18-29 (1)	100.0% (1)	-	-	-	-	-	
30-39 (8)	75.0% (6)	12.5% (1)	-	12.5% (1)	-	-	
40-49 (14)	71.4% (10)	14.3% (2)	-	14.3% (2)	-	-	
50-59 (21)	85.7% (18)	9.5% (2)	-	4.8% (1)	-	-	
60-69 (29)	89.7% (26)	3.4% (1)	3.4% (1)	3.4% (1)	-	-	
70-79 (18)	83.3% (15)	-	-	11.1% (2)	-	5.6% (1)	
80-89 (3)	66.7% (2)	-	-	33.3% (1)	-	-	
unknown (16)	68.8% (11)	-	-	25.0% (4)	6.3% (1)	-	

CITY WARD							110 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	
Unknown (110)	80.9% (89)	5.5% (6)	0.9% (1)	10.9% (12)	0.9% (1)	0.9% (1)	

VOTERS GENDER							110 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	
F (65)	80.0% (52)	4.6% (3)	1.5% (1)	10.8% (7)	1.5% (1)	1.5% (1)	
M (44)	81.8% (36)	6.8% (3)	-	11.4% (5)	-	-	
Unknown (1)	100.0% (1)	-	-	-	-	-	

## Your home: Do you?

CURRENT RESULTS		176 Total Responses	
<b>A</b>	Own (164)	<div style="width: 93%;"><div style="width: 93%;"></div></div>	93% (164)
<b>B</b>	Rent (12)	<div style="width: 7%;"><div style="width: 7%;"></div></div>	7% (12)

REGISTERED VS NON-REGISTERED			
	<b>A</b>	<b>B</b>	
Registered Voters (106)	93.4% (99)	6.6% (7)	
Non-Registered Voters (70)	92.9% (65)	7.1% (5)	

ALL RESPONDENTS			
	<b>A</b>	<b>B</b>	
All respondents (176)	93.0% (164)	7.0% (12)	
Registered Voters in Hudson, WI, WI (106)	93.4% (99)	6.6% (7)	
Live in Hudson, WI, WI (168) - Self-reported	92.9% (156)	7.1% (12)	
Subscribers to Hudson, WI, WI (168)	92.9% (156)	7.1% (12)	
Register respondents from anywhere (111)	94.0% (104)	6.0% (7)	

AGE RANGE				111 REGISTERED VOTERS
	<b>A</b>	<b>B</b>		
18-29 (1)	100.0% (1)	-		
30-39 (9)	77.8% (7)	22.2% (2)		
40-49 (14)	100.0% (14)	-		
50-59 (21)	95.2% (20)	4.8% (1)		
60-69 (29)	96.6% (28)	3.4% (1)		
70-79 (18)	94.4% (17)	5.6% (1)		
80-89 (3)	66.7% (2)	33.3% (1)		
unknown (16)	93.8% (15)	6.3% (1)		

CITY WARD				111 REGISTERED VOTERS
	<b>A</b>	<b>B</b>		
Unknown (111)	93.7% (104)	6.3% (7)		

VOTERS GENDER				111 REGISTERED VOTERS
	<b>A</b>	<b>B</b>		
F (66)	90.9% (60)	9.1% (6)		
M (44)	97.7% (43)	2.3% (1)		
Unknown (1)	100.0% (1)	-		

## Level of Education: Which best applies to you?



**REGISTERED VS NON-REGISTERED**

	A	B	C	D	E	F
Registered Voters (106)	-	2.8% (3)	9.4% (10)	9.4% (10)	39.6% (42)	38.7% (41)
Non-Registered Voters (70)	1.4% (1)	7.1% (5)	10.0% (7)	7.1% (5)	31.4% (22)	42.9% (30)

**ALL RESPONDENTS**

	A	B	C	D	E	F
All respondents (176)	1.0% (1)	5.0% (8)	10.0% (17)	9.0% (15)	36.0% (64)	40.0% (71)
Registered Voters in Hudson, WI, WI (106)	-	2.8% (3)	9.4% (10)	9.4% (10)	39.6% (42)	38.7% (41)
Live in Hudson, WI, WI (168) - Self-reported	0.6% (1)	4.8% (8)	10.1% (17)	8.9% (15)	36.9% (62)	38.7% (65)
Subscribers to Hudson, WI, WI (168)	0.6% (1)	4.8% (8)	10.1% (17)	8.9% (15)	36.9% (62)	38.7% (65)
Register respondents from anywhere (111)	-	3.0% (3)	9.0% (10)	9.0% (10)	38.0% (42)	41.0% (46)

**AGE RANGE** 111 REGISTERED VOTERS

	A	B	C	D	E	F
18-29 (1)	-	-	-	-	100.0% (1)	-
30-39 (9)	-	-	-	-	66.7% (6)	33.3% (3)
40-49 (14)	-	7.1% (1)	21.4% (3)	14.3% (2)	35.7% (5)	21.4% (3)
50-59 (21)	-	-	9.5% (2)	9.5% (2)	42.9% (9)	38.1% (8)
60-69 (29)	-	-	10.3% (3)	20.7% (6)	24.1% (7)	44.8% (13)
70-79 (18)	-	5.6% (1)	5.6% (1)	-	27.8% (5)	61.1% (11)
80-89 (3)	-	-	33.3% (1)	-	66.7% (2)	-
unknown (16)	-	6.3% (1)	-	-	43.8% (7)	50.0% (8)

**CITY WARD** 111 REGISTERED VOTERS

	A	B	C	D	E	F
Unknown (111)	-	2.7% (3)	9.0% (10)	9.0% (10)	37.8% (42)	41.4% (46)

**VOTERS GENDER** 111 REGISTERED VOTERS

	A	B	C	D	E	F
F (66)	-	3.0% (2)	9.1% (6)	10.6% (7)	37.9% (25)	39.4% (26)
M (44)	-	2.3% (1)	9.1% (4)	6.8% (3)	38.6% (17)	43.2% (19)
Unknown (1)	-	-	-	-	-	100.0% (1)

## Employment Situation: Which best applies to you?



**REGISTERED VS NON-REGISTERED**

	A	B	C	D	E	F
Registered Voters (106)	49.1% (52)	9.4% (10)	1.9% (2)	34.0% (36)	5.7% (6)	-
Non-Registered Voters (70)	54.3% (38)	12.9% (9)	-	25.7% (18)	2.9% (2)	4.3% (3)

**ALL RESPONDENTS**

	A	B	C	D	E	F
All respondents (176)	51.0% (90)	11.0% (19)	1.0% (2)	31.0% (54)	5.0% (8)	2.0% (3)
Registered Voters in Hudson, WI, WI (106)	49.1% (52)	9.4% (10)	1.9% (2)	34.0% (36)	5.7% (6)	-
Live in Hudson, WI, WI (168) - Self-reported	52.4% (88)	11.3% (19)	1.2% (2)	28.6% (48)	4.8% (8)	1.8% (3)
Subscribers to Hudson, WI, WI (168)	52.4% (88)	11.3% (19)	1.2% (2)	28.6% (48)	4.8% (8)	1.8% (3)
Register respondents from anywhere (111)	48.0% (53)	9.0% (10)	2.0% (2)	36.0% (40)	5.0% (6)	-

**AGE RANGE** 111 REGISTERED VOTERS

	A	B	C	D	E	F
18-29 (1)	100.0% (1)	-	-	-	-	-
30-39 (9)	66.7% (6)	11.1% (1)	-	11.1% (1)	11.1% (1)	-
40-49 (14)	57.1% (8)	21.4% (3)	-	21.4% (3)	-	-
50-59 (21)	81.0% (17)	4.8% (1)	4.8% (1)	-	9.5% (2)	-
60-69 (29)	34.5% (10)	10.3% (3)	-	55.2% (16)	-	-
70-79 (18)	5.6% (1)	5.6% (1)	-	88.9% (16)	-	-
80-89 (3)	33.3% (1)	-	33.3% (1)	33.3% (1)	-	-
unknown (16)	56.3% (9)	6.3% (1)	-	18.8% (3)	18.8% (3)	-

**CITY WARD** 111 REGISTERED VOTERS

	A	B	C	D	E	F
Unknown (111)	47.7% (53)	9.0% (10)	1.8% (2)	36.0% (40)	5.4% (6)	-

**VOTERS GENDER** 111 REGISTERED VOTERS

	A	B	C	D	E	F
F (66)	43.9% (29)	12.1% (8)	3.0% (2)	33.3% (22)	7.6% (5)	-
M (44)	52.3% (23)	4.5% (2)	-	40.9% (18)	2.3% (1)	-
Unknown (1)	100.0% (1)	-	-	-	-	-

## What age range are you in?



**REGISTERED VS NON-REGISTERED**

	A	B	C	D	E	F	G	H
Registered Voters (106)	-	0.9% (1)	8.5% (9)	17.0% (18)	20.8% (22)	23.6% (25)	21.7% (23)	7.5% (8)
Non-Registered Voters (70)	1.4% (1)	2.9% (2)	10.0% (7)	11.4% (8)	30.0% (21)	25.7% (18)	12.9% (9)	5.7% (4)

**ALL RESPONDENTS**

	A	B	C	D	E	F	G	H
All respondents (176)	1.0% (1)	2.0% (3)	9.0% (16)	15.0% (26)	24.0% (43)	24.0% (43)	18.0% (32)	7.0% (12)
Registered Voters in Hudson, WI, WI (106)	-	0.9% (1)	8.5% (9)	17.0% (18)	20.8% (22)	23.6% (25)	21.7% (23)	7.5% (8)
Live in Hudson, WI, WI (168) - Self-reported	0.6% (1)	1.8% (3)	9.5% (16)	15.5% (26)	25.0% (42)	24.4% (41)	16.7% (28)	6.5% (11)
Subscribers to Hudson, WI, WI (168)	0.6% (1)	1.8% (3)	9.5% (16)	15.5% (26)	25.0% (42)	24.4% (41)	16.7% (28)	6.5% (11)
Register respondents from anywhere (111)	-	1.0% (1)	8.0% (9)	16.0% (18)	20.0% (22)	24.0% (27)	23.0% (25)	8.0% (9)

**AGE RANGE** 111 REGISTERED VOTERS

Age Range	A	B	C	D	E	F	G	H
18-29 (1)	-	-	-	100.0% (1)	-	-	-	-
30-39 (9)	-	-	44.4% (4)	44.4% (4)	-	-	11.1% (1)	-
40-49 (14)	-	-	-	35.7% (5)	50.0% (7)	7.1% (1)	-	7.1% (1)
50-59 (21)	-	-	4.8% (1)	-	57.1% (12)	38.1% (8)	-	-
60-69 (29)	-	-	-	6.9% (2)	-	51.7% (15)	41.4% (12)	-
70-79 (18)	-	-	-	-	-	-	61.1% (11)	38.9% (7)
80-89 (3)	-	33.3% (1)	-	33.3% (1)	-	-	-	33.3% (1)
unknown (16)	-	-	25.0% (4)	31.3% (5)	18.8% (3)	18.8% (3)	6.3% (1)	-

**CITY WARD** 111 REGISTERED VOTERS

City Ward	A	B	C	D	E	F	G	H
Unknown (111)	-	0.9% (1)	8.1% (9)	16.2% (18)	19.8% (22)	24.3% (27)	22.5% (25)	8.1% (9)

**VOTERS GENDER** 111 REGISTERED VOTERS

Gender	A	B	C	D	E	F	G	H
F (66)	-	1.5% (1)	9.1% (6)	18.2% (12)	21.2% (14)	22.7% (15)	21.2% (14)	6.1% (4)
M (44)	-	-	4.5% (2)	13.6% (6)	18.2% (8)	27.3% (12)	25.0% (11)	11.4% (5)
Unknown (1)	-	-	100.0% (1)	-	-	-	-	-

## What is your gender?

CURRENT RESULTS		176 Total Responses	
<b>A</b>	Male (70)	<div style="width: 40%;"></div>	40% (70)
<b>B</b>	Female (105)	<div style="width: 60%;"></div>	60% (105)
<b>C</b>	Other: (Please specify in next question) (1)	<div style="width: 1%;"></div>	1% (1)

REGISTERED VS NON-REGISTERED			
	<b>A</b>	<b>B</b>	<b>C</b>
Registered Voters (106)	40.6% (43)	58.5% (62)	0.9% (1)
Non-Registered Voters (70)	38.6% (27)	61.4% (43)	-

ALL RESPONDENTS			
	<b>A</b>	<b>B</b>	<b>C</b>
All respondents (176)	40.0% (70)	60.0% (105)	1.0% (1)
Registered Voters in Hudson, WI, WI (106)	40.6% (43)	58.5% (62)	0.9% (1)
Live in Hudson, WI, WI (168) - Self-reported	39.9% (67)	59.5% (100)	0.6% (1)
Subscribers to Hudson, WI, WI (168)	39.9% (67)	59.5% (100)	0.6% (1)
Register respondents from anywhere (111)	41.0% (45)	59.0% (65)	1.0% (1)

AGE RANGE				111 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	
18-29 (1)	-	100.0% (1)	-	
30-39 (9)	33.3% (3)	66.7% (6)	-	
40-49 (14)	57.1% (8)	42.9% (6)	-	
50-59 (21)	28.6% (6)	66.7% (14)	4.8% (1)	
60-69 (29)	48.3% (14)	51.7% (15)	-	
70-79 (18)	50.0% (9)	50.0% (9)	-	
80-89 (3)	-	100.0% (3)	-	
unknown (16)	31.3% (5)	68.8% (11)	-	

CITY WARD				111 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	
Unknown (111)	40.5% (45)	58.6% (65)	0.9% (1)	

VOTERS GENDER				111 REGISTERED VOTERS
	<b>A</b>	<b>B</b>	<b>C</b>	
F (66)	3.0% (2)	95.5% (63)	1.5% (1)	
M (44)	95.5% (42)	4.5% (2)	-	
Unknown (1)	100.0% (1)	-	-	

If you responded "Other" in Question 25, please specify here.

---



Anonymous user's Opinion

own - you've likely got your numbers off.



Anonymous user's Opinion

It shouldn't matter



Anonymous user's Opinion

What The??? Do we really have to be this liberal?



Anonymous user's Opinion

NA

## What is your household income range? (Optional Question)



**REGISTERED VS NON-REGISTERED**

	A	B	C	D	E	F	G	H
Registered Voters (90)	2.2% (2)	1.1% (1)	3.3% (3)	8.9% (8)	23.3% (21)	35.6% (32)	14.4% (13)	11.1% (10)
Non-Registered Voters (61)	1.6% (1)	6.6% (4)	3.3% (2)	8.2% (5)	19.7% (12)	24.6% (15)	21.3% (13)	14.8% (9)

**ALL RESPONDENTS**

	A	B	C	D	E	F	G	H
All respondents (151)	2.0% (3)	3.0% (5)	3.0% (5)	9.0% (13)	22.0% (33)	31.0% (47)	17.0% (26)	13.0% (19)
Registered Voters in Hudson, WI, WI (90)	2.2% (2)	1.1% (1)	3.3% (3)	8.9% (8)	23.3% (21)	35.6% (32)	14.4% (13)	11.1% (10)
Live in Hudson, WI, WI (144) - Self-reported	2.1% (3)	3.5% (5)	3.5% (5)	7.6% (11)	22.2% (32)	30.6% (44)	18.1% (26)	12.5% (18)
Subscribers to Hudson, WI, WI (144)	2.1% (3)	3.5% (5)	3.5% (5)	7.6% (11)	22.2% (32)	30.6% (44)	18.1% (26)	12.5% (18)
Register respondents from anywhere (94)	2.0% (2)	1.0% (1)	3.0% (3)	10.0% (9)	22.0% (21)	37.0% (35)	14.0% (13)	11.0% (10)

**AGE RANGE** 94 REGISTERED VOTERS

Age Range	A	B	C	D	E	F	G	H
18-29 (1)	-	-	-	-	-	-	100.0% (1)	-
30-39 (8)	-	-	-	25.0% (2)	25.0% (2)	25.0% (2)	25.0% (2)	-
40-49 (13)	-	-	-	15.4% (2)	15.4% (2)	38.5% (5)	23.1% (3)	7.7% (1)
50-59 (17)	-	-	-	-	17.6% (3)	47.1% (8)	5.9% (1)	29.4% (5)
60-69 (26)	-	-	-	11.5% (3)	19.2% (5)	38.5% (10)	19.2% (5)	11.5% (3)
70-79 (14)	7.1% (1)	7.1% (1)	14.3% (2)	7.1% (1)	28.6% (4)	35.7% (5)	-	-
80-89 (2)	50.0% (1)	-	-	-	50.0% (1)	-	-	-
unknown (13)	-	-	7.7% (1)	7.7% (1)	30.8% (4)	38.5% (5)	7.7% (1)	7.7% (1)

**CITY WARD** 94 REGISTERED VOTERS

City Ward	A	B	C	D	E	F	G	H
Unknown (94)	2.1% (2)	1.1% (1)	3.2% (3)	9.6% (9)	22.3% (21)	37.2% (35)	13.8% (13)	10.6% (10)

**VOTERS GENDER** 94 REGISTERED VOTERS

Gender	A	B	C	D	E	F	G	H
F (55)	3.6% (2)	1.8% (1)	3.6% (2)	14.5% (8)	20.0% (11)	32.7% (18)	9.1% (5)	14.5% (8)
M (38)	-	-	2.6% (1)	2.6% (1)	23.7% (9)	44.7% (17)	21.1% (8)	5.3% (2)
Unknown (1)	-	-	-	-	100.0% (1)	-	-	-

## What category best describes you?

CURRENT RESULTS		172 Total Responses
<b>A</b>	White (167)	97% (167)
<b>B</b>	Hispanic, Latino or Spanish origin (3)	2% (3)
<b>C</b>	Black or African American (0)	0% (0)
<b>D</b>	Asian (2)	1% (2)
<b>E</b>	American Indian or Alaska Native (2)	1% (2)
<b>F</b>	Middle Eastern or North African (0)	0% (0)
<b>G</b>	Native Hawaiian or Other Pacific Islander (0)	0% (0)
<b>H</b>	Some other race, ethnicity, or origin (3)	2% (3)

### REGISTERED VS NON-REGISTERED

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>
Registered Voters (105)	97.1% (102)	1.0% (1)	-	1.9% (2)	-	-	-	1.0% (1)
Non-Registered Voters (67)	97.0% (65)	3.0% (2)	-	-	3.0% (2)	-	-	3.0% (2)

### ALL RESPONDENTS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>
All respondents (172)	97.0% (167)	2.0% (3)	-	1.0% (2)	1.0% (2)	-	-	2.0% (3)
Registered Voters in Hudson, WI, WI (105)	97.1% (102)	1.0% (1)	-	1.9% (2)	-	-	-	1.0% (1)
Live in Hudson, WI, WI (165) - Self-reported	97.0% (160)	1.2% (2)	-	1.2% (2)	0.6% (1)	-	-	1.8% (3)
Subscribers to Hudson, WI, WI (165)	97.0% (160)	1.2% (2)	-	1.2% (2)	0.6% (1)	-	-	1.8% (3)
Register respondents from anywhere (109)	97.0% (106)	2.0% (2)	-	2.0% (2)	1.0% (1)	-	-	1.0% (1)

### AGE RANGE

112 REGISTERED VOTERS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>
18-29 (1)	100.0% (1)	-	-	-	-	-	-	-
30-39 (9)	100.0% (9)	-	-	11.1% (1)	-	-	-	-
40-49 (14)	92.9% (13)	7.1% (1)	-	-	7.1% (1)	-	-	7.1% (1)
50-59 (20)	100.0% (20)	-	-	-	-	-	-	-
60-69 (29)	96.6% (28)	-	-	3.4% (1)	-	-	-	-
70-79 (17)	100.0% (17)	-	-	-	-	-	-	-
80-89 (3)	100.0% (3)	-	-	-	-	-	-	-
unknown (16)	93.8% (15)	6.3% (1)	-	-	-	-	-	-

### CITY WARD

112 REGISTERED VOTERS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>
Unknown (109)	97.2% (106)	1.8% (2)	-	1.8% (2)	0.9% (1)	-	-	0.9% (1)

### VOTERS GENDER

112 REGISTERED VOTERS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>
F (64)	98.4% (63)	-	-	3.1% (2)	-	-	-	-
M (44)	95.5% (42)	4.5% (2)	-	-	2.3% (1)	-	-	2.3% (1)
Unknown (1)	100.0% (1)	-	-	-	-	-	-	-

## How would you like to participate in this process?



**REGISTERED VS NON-REGISTERED**

	A	B	C	D	E	F	G	H
Registered Voters (105)	59.0% (62)	21.9% (23)	20.0% (21)	19.0% (20)	89.5% (94)	76.2% (80)	40.0% (42)	59.0% (62)
Non-Registered Voters (68)	61.8% (42)	17.6% (12)	19.1% (13)	11.8% (8)	91.2% (62)	73.5% (50)	36.8% (25)	52.9% (36)

**ALL RESPONDENTS**

	A	B	C	D	E	F	G	H
All respondents (173)	60.0% (104)	20.0% (35)	20.0% (34)	16.0% (28)	90.0% (156)	75.0% (130)	39.0% (67)	57.0% (98)
Registered Voters in Hudson, WI, WI (105)	59.0% (62)	21.9% (23)	20.0% (21)	19.0% (20)	89.5% (94)	76.2% (80)	40.0% (42)	59.0% (62)
Live in Hudson, WI, WI (166) - Self-reported	60.8% (101)	20.5% (34)	19.9% (33)	16.3% (27)	91.0% (151)	77.1% (128)	39.2% (65)	57.8% (96)
Subscribers to Hudson, WI, WI (166)	60.8% (101)	20.5% (34)	19.9% (33)	16.3% (27)	91.0% (151)	77.1% (128)	39.2% (65)	57.8% (96)
Register respondents from anywhere (109)	58.0% (63)	21.0% (23)	20.0% (22)	18.0% (20)	89.0% (97)	74.0% (81)	40.0% (44)	59.0% (64)

**AGE RANGE** 414 REGISTERED VOTERS

	A	B	C	D	E	F	G	H
18-29 (1)	100.0% (1)	-	-	-	100.0% (1)	-	-	100.0% (1)
30-39 (9)	55.6% (5)	11.1% (1)	11.1% (1)	11.1% (1)	100.0% (9)	88.9% (8)	55.6% (5)	55.6% (5)
40-49 (14)	64.3% (9)	21.4% (3)	14.3% (2)	14.3% (2)	100.0% (14)	78.6% (11)	21.4% (3)	71.4% (10)
50-59 (20)	60.0% (12)	10.0% (2)	15.0% (3)	10.0% (2)	90.0% (18)	60.0% (12)	50.0% (10)	60.0% (12)
60-69 (29)	58.6% (17)	41.4% (12)	41.4% (12)	41.4% (12)	96.6% (28)	82.8% (24)	44.8% (13)	58.6% (17)
70-79 (17)	41.2% (7)	23.5% (4)	11.8% (2)	11.8% (2)	58.8% (10)	64.7% (11)	41.2% (7)	52.9% (9)
80-89 (3)	100.0% (3)	33.3% (1)	-	-	100.0% (3)	66.7% (2)	33.3% (1)	66.7% (2)
unknown (16)	56.3% (9)	-	12.5% (2)	6.3% (1)	87.5% (14)	81.3% (13)	31.3% (5)	50.0% (8)

**CITY WARD** 414 REGISTERED VOTERS

	A	B	C	D	E	F	G	H
Unknown (109)	57.8% (63)	21.1% (23)	20.2% (22)	18.3% (20)	89.0% (97)	74.3% (81)	40.4% (44)	58.7% (64)

**VOTERS GENDER** 414 REGISTERED VOTERS

	A	B	C	D	E	F	G	H
F (64)	59.4% (38)	17.2% (11)	15.6% (10)	17.2% (11)	85.9% (55)	75.0% (48)	43.8% (28)	59.4% (38)
M (44)	54.5% (24)	27.3% (12)	27.3% (12)	20.5% (9)	93.2% (41)	72.7% (32)	36.4% (16)	56.8% (25)
Unknown (1)	100.0% (1)	-	-	-	100.0% (1)	100.0% (1)	-	100.0% (1)

## Is there anything else you would like to share with us?

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**Anonymous user's Opinion**



1. Discourage excessive landscaping and irrigation: I have lived in one association and now am building in another. Both associations require homeowners to have in ground irrigation. This should be discouraged by city leaders. We have residents who have been indoctrinated to believe that their home value is tied to having every blade of grass green and chemical fertilizers applied monthly. 2) We are aging: Consider policies that support seniors. A senior center & dementia friendly policies are needed. 3) Transportation: Consider ways to get people off of their dependence on single drivers in cars. 4) Businesses "on the hill" are not walk-able. People drive too fast and don't look for pedestrians.

**Anonymous user's Opinion**



Need a more forward thinking mayor!

**Anonymous user's Opinion**



Love the inclusion activities! We need to be a diverse community united in our efforts to make life better for all.

**Anonymous user's Opinion**



Get the women cheaters out of this town before they corrupt it and we get a bad rap.

**Anonymous user's Opinion**



Thank you!

**Anonymous user's Opinion**



Encourage and welcome diversity in the community

**Anonymous user's Opinion**



There should be light rail service between Hudson and the Twin Cities. Build it right in the middle of I-94.

**Anonymous user's Opinion**



The new parking system is going to stop a bunch of people (especially elderly) from coming down town. Need more options besides using phone.

**Anonymous user's Opinion**



Highly concerned with the enormous apartment complex-you are destroying everyone's property value! And the horrible idea of placing a 5 story building on Main Street! And Do NOT remove memorials!

**Anonymous user's Opinion**



I'm disappointed in the free parking downtown being turned into metered parking.

**Anonymous user's Opinion**



How can you allow the garbage hauler to increase their rates 30%? People on fixed incomes cannot afford that. Who gets a 30% increase in SS or wages? Greedy business.

**Anonymous user's Opinion**



Thank you for the important work you do.

**Anonymous user's Opinion**



Please lower the cost of water for irrigation purposes. It is too expensive at the current rate! Please also no more increases to taxes!! We are already paying way too much in taxes!!

**Anonymous user's Opinion**



Much more transparency is needed from the city instead of saying one thing and then doing another behind closed doors. The library and the School remodels were great examples of poor communication and decisions made even after the public voted differently.

**Anonymous user's Opinion**



Diversity in ethnicity, religion, and income levels are worth embracing; let's get ahead of the coming changes in demographics and prepare Hudson to be a more welcoming community.

**Anonymous user's Opinion**



Keep the single family residential zoning down town ( First, Second, and Third Streets from Dairy Queen on the south to Division St. on the north ) single family or at most duplex.

**Anonymous user's Opinion**



N/A

**Anonymous user's Opinion**



Hudson is a beautiful town but it lacks diversity and lacks an open political dialogue.

**Anonymous user's Opinion**



I fear that Hudson downtown is losing its charm by heading down the route of Stillwater with a downtown that primarily benefits restaurants and visitors and not serving the needs of its residents.

**Anonymous user's Opinion**



Yes- people move here as its 'country'. If you keep building HUGE apartment buildings, adding Chic-Filets and the like, the entire point of living here is gone. If people want high density and a strip mall on every corner- they should really relocate to Woodbury! In other words- maintain the charm and character of this town! Having lived in major Cities (Chicago, Twin Cities), well, I relocated here as its NOT busy/chaotic/crime ridden as a major City. I cannot emphasize enough how important it is to maintain the small town atmosphere here, else, there's really no point in moving here as it'll be a carbon copy of Woodbury.

**Anonymous user's Opinion**



Our elected officials never seem to have time to meet with the regular folks to discuss their concerns. Years ago you could have coffee with the mayor or whom ever and they listened. Not anymore.

**Anonymous user's Opinion**



No

**Anonymous user's Opinion**



I ♥ Hudson.

**Anonymous user's Opinion**



Thank you for starting the conversation. I hope there is a plan to seinput from all stakeholder groups. I happened upon this based on a friend's comment on Facebook. Expecting people to come to your website provide very important feedback is going significantly limit the diversity of responses. What organizations have you reached out to for help in letting people know about this. The HSO reaches fewer and fewer people each year. The school district could share this survey link, businesses with their employees, are hard copies in the places people gather (restaurants, YMCA), post a notice at the parks about this opportunity.

**Anonymous user's Opinion**



Transportation within the city and to surrounding communities will become more important. Within 20 years, county communities will be growing physically closer together. We will need to promote mass transit opportunities and plan for it now.

**Anonymous user's Opinion**



The new streetlights on the extension of Ward Ave by the new fire station and along Hillcrest Drive have never been turned on. This a public safety disaster waiting to happen!

**Anonymous user's Opinion**



We need more low income housing. The rental market is too high for our service people to live close to their jobs and this will hurt us as a community when service standards become subpar.

**Anonymous user's Opinion**



Aware of and NOT in favor of the additional industrial rezoning of the Hartman property at hwy 12 and city rd U. There are plenty of industrial lots available within the existing industrial parks.

**Anonymous user's Opinion**



Hudson needs to welcome more diversity.

**Anonymous user's Opinion**



I do not want to see Hudson become the next Woodbury

**Anonymous user's Opinion**



no.

**Anonymous user's Opinion**



? I am glad you are reaching out to the public. Too often development decisions seem to have been left to the good old boys in the past.

**Anonymous user's Opinion**



The diked road rocks are unstable and a hazard.

**Anonymous user's Opinion**



Hudson needs a plan that moves the city to environmental sustainability asap. The climate crisis is real!!! I see where cities like Eau Claire have already taken major steps in that direction. We need goals and objectives that are in alignment with scientifically sound conservation practices and then we need plans that will move us toward accomplishment of them in an efficient and effective manner. Without a healthy environment nothing much else is important.

**Anonymous user's Opinion**



I'd like to see Hudson go on record as a welcoming city with rainbow flags on mainstream.

**Anonymous user's Opinion**



Thanks for asking our opinion!

**Anonymous user's Opinion**



No

**Anonymous user's Opinion**



As a new resident I am excited to learn about the future development and vision for Hudson.

-  **Anonymous user's Opinion**  
We live in a river town and there is no trailer parking on streets. I live in third street and any one can park their car in front of my house for long periods. We can't leave a boat trailer parked on our street for the day while we boat on the river when boat launch is full. People come to boat eat,shop and you take away right to park on streets.
-  **Anonymous user's Opinion**  
Perhaps reach broader audience with online public meetings. In the style of say webex or other similar service.
-  **Anonymous user's Opinion**  
Thank you for asking for our feedback
-  **Anonymous user's Opinion**  
Please just keep the communication through many channels. I only heard about this cause someone posted it on NextDoor.
-  **Anonymous user's Opinion**  
I own property in the city of Hudson but reside in the town of Hudson.
-  **Anonymous user's Opinion**  
We need to encourage younger people to get involved in local "politics." Not just the ones with PC agendas that the schools promote.
-  **Anonymous user's Opinion**  
I appreciate your efforts to include public input in the planning for the areas future.
-  **Anonymous user's Opinion**  
Encourage Hudson to be a walkable, friendly community.
-  **Anonymous user's Opinion**  
Be sure to provide updates, events, new developments, etc in the local newspaper. I'd like to know more about the plans for the Carmichael and Vine project as well. How are Vine and Carmichael changing to support the new traffic?
-  **Anonymous user's Opinion**  
I appreciate this commitment to make improvements.
-  **Anonymous user's Opinion**  
I want to see Hudson take significant steps to address the impact of climate change on our local environment, businesses and city budget. I also want Hudson to make changes to become more inclusive of people of different ethnic, racial and socio-economic backgrounds.
-  **Anonymous user's Opinion**  
parking, parking, parking. Thanks.
-  **Anonymous user's Opinion**  
thank you and I hope there is going to be a reply or feedback from all the questions that have been asked. More forums for cultural Exchange. Very little diversity cultural populations in the area in general. Too White and Republican.
-  **Anonymous user's Opinion**  
I would like to see a moratorium on building large houses on large lots that young people can't afford and an expansion of affordable housing. My kids getting out of college can't afford to live here. The only jobs in this town for young people are a restaurants.
-  **Anonymous user's Opinion**  
bury utilities, get mailboxes off the curb, have walkable sidewalks
-  **Anonymous user's Opinion**  
Hudson needs to Aggressively expand Housing east of Carmichael and south of FF.
-  **Anonymous user's Opinion**  
PLEASE DO NOT ANNEX OR TOUCH THE COUNTY LAND BEHIND THE GOVERNMENT CENTER. ITS ALL WE HAVE LEFT OF OPEN SPACE IN THE CITY AREA.
-  **Anonymous user's Opinion**  
I love living here.
-  **Anonymous user's Opinion**  
Hudson is a wonderful community, thank you for your service and time in keeping it great.
-  **Anonymous user's Opinion**  
Please follow the zoning committee's findings that say the Coulee road strip from Main Street up to the light should not house a huge housing project. Please keep that green space open. We don't need a concrete jungle. Please slow down on commercial building as well - there are so many open retail and commercial properties. It's unattractive. Also - please keep supporting community oriented organizations like the Phipps and the St. Croix Sailing School - these treasures bring so many people over the bridge who then discover the charm of Hudson and in turn spend money here which supports all of us - and that's the whole goal right? Once they get here though, it's important to make them feel comfortable and welcome. I don't believe the new parking system is accomplishing that. The old

meters were charming and user friendly for all ages.



**Anonymous user's Opinion**

There should be a limit on the development of the City as far as annexing land. There is a cost that is associated with adding land and streets (infrastructure) that need to be replaced and maintained, along with protective services, School cost, transportation.....



**Anonymous user's Opinion**

I enjoy the questionnaire. Being able to give input on something without attending a public meeting.



**Anonymous user's Opinion**

By asking these questions, you demonstrate your sincere interest in hearing what constituents have to say. Thanks for being progressive enough to ask our opinions.



**Anonymous user's Opinion**

I would anticipate and expect a community of the caliber of Hudson to set very high standards of development and appearance as well as access. This clearly has not happened on the "hill" it is a mishmash of poor development, design and standards. Woodbury can set high standards and it has worked. Hudson can do as well if not better.

## How did you hear about this survey?

CURRENT RESULTS			129 Total Responses
<b>A</b>	Newspaper (15)		12% (15)
<b>B</b>	River Channel (2)		2% (2)
<b>C</b>	Flyer for the Project (3)		2% (3)
<b>D</b>	Someone Told Me About It (30)		23% (30)
<b>E</b>	City Website (10)		8% (10)
<b>F</b>	Facebook (46)		36% (46)
<b>G</b>	Twitter (0)		0% (0)
<b>H</b>	Email (15)		12% (15)
<b>I</b>	Other (30)		23% (30)

### REGISTERED VS NON-REGISTERED

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>
Registered Voters (75)	17.3% (13)	2.7% (2)	1.3% (1)	21.3% (16)	9.3% (7)	33.3% (25)	-	16.0% (12)	21.3% (16)
Non-Registered Voters (54)	3.7% (2)	-	3.7% (2)	25.9% (14)	5.6% (3)	38.9% (21)	-	5.6% (3)	25.9% (14)

### ALL RESPONDENTS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>
All respondents (129)	12.0% (15)	2.0% (2)	2.0% (3)	23.0% (30)	8.0% (10)	36.0% (46)	-	12.0% (15)	23.0% (30)
Registered Voters in Hudson, WI, WI (75)	17.3% (13)	2.7% (2)	1.3% (1)	21.3% (16)	9.3% (7)	33.3% (25)	-	16.0% (12)	21.3% (16)
Live in Hudson, WI, WI (125) - Self-reported	12.0% (15)	1.6% (2)	2.4% (3)	22.4% (28)	7.2% (9)	36.8% (46)	-	11.2% (14)	24.0% (30)
Subscribers to Hudson, WI, WI (125)	12.0% (15)	1.6% (2)	2.4% (3)	22.4% (28)	7.2% (9)	36.8% (46)	-	11.2% (14)	24.0% (30)
Register respondents from anywhere (77)	17.0% (13)	3.0% (2)	1.0% (1)	23.0% (18)	9.0% (7)	32.0% (25)	-	16.0% (12)	21.0% (16)

### AGE RANGE

94 REGISTERED VOTERS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>
18-29 (1)	-	-	-	-	-	100.0% (1)	-	-	-
30-39 (6)	-	-	-	-	-	66.7% (4)	-	-	33.3% (2)
40-49 (7)	-	-	14.3% (1)	14.3% (1)	-	57.1% (4)	-	14.3% (1)	14.3% (1)
50-59 (15)	33.3% (5)	-	-	46.7% (7)	6.7% (1)	26.7% (4)	-	6.7% (1)	20.0% (3)
60-69 (24)	25.0% (6)	-	-	25.0% (6)	16.7% (4)	20.8% (5)	-	20.8% (5)	25.0% (6)
70-79 (12)	-	16.7% (2)	-	33.3% (4)	8.3% (1)	33.3% (4)	-	16.7% (2)	-
80-89 (1)	-	-	-	-	-	100.0% (1)	-	-	-
unknown (11)	18.2% (2)	-	-	-	9.1% (1)	18.2% (2)	-	27.3% (3)	36.4% (4)

### CITY WARD

94 REGISTERED VOTERS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>
Unknown (77)	16.9% (13)	2.6% (2)	1.3% (1)	23.4% (18)	9.1% (7)	32.5% (25)	-	15.6% (12)	20.8% (16)

### VOTERS GENDER

94 REGISTERED VOTERS

	<b>A</b>	<b>B</b>	<b>C</b>	<b>D</b>	<b>E</b>	<b>F</b>	<b>G</b>	<b>H</b>	<b>I</b>
F (44)	13.6% (6)	2.3% (1)	-	29.5% (13)	6.8% (3)	40.9% (18)	-	9.1% (4)	22.7% (10)
M (32)	21.9% (7)	3.1% (1)	3.1% (1)	15.6% (5)	12.5% (4)	21.9% (7)	-	21.9% (7)	18.8% (6)
Unknown (1)	-	-	-	-	-	-	-	100.0% (1)	-

**If you responded "Other" in question 31, please where you heard about this survey.**

---

 **Anonymous user's Opinion**  
Polco

 **Anonymous user's Opinion**  
Attended the first public meeting.

 **Anonymous user's Opinion**  
NextDoor community web site

 **Anonymous user's Opinion**  
Nextdoor website

 **Anonymous user's Opinion**  
NextDoor app

 **Anonymous user's Opinion**  
Next Door App

 **Anonymous user's Opinion**  
NextDoor.com

 **Anonymous user's Opinion**  
Next door app

 **Anonymous user's Opinion**  
Nextdoor Neighbor social website

 **Anonymous user's Opinion**  
nextdoor.com

 **Anonymous user's Opinion**  
Neighborhood App

 **Anonymous user's Opinion**  
Nextdoor app

 **Anonymous user's Opinion**  
NextDoor

 **Anonymous user's Opinion**  
Nextdoor app

 **Anonymous user's Opinion**  
Nextdoor digest River's Edge

 **Anonymous user's Opinion**  
Nextdoor App

 **Anonymous user's Opinion**  
Nextdoor Digest

 **Anonymous user's Opinion**  
Nextdoor (dot) com

 **Anonymous user's Opinion**  
Next-door Digest Sadly, our local newspaper is no longer covering the details of Hudson. For instance, I was unaware of the November 21 meeting.

 **Anonymous user's Opinion**

Nextdoor

 **Anonymous user's Opinion**

<https://nextdoor.com/>

 **Anonymous user's Opinion**

Nextdoor

 **Anonymous user's Opinion**

Nextdoor River's Edge website.

 **Anonymous user's Opinion**

Watching City Council meeting

 **Anonymous user's Opinion**

The newspaper no longer services Hudson. It lake information. A community Nextdoor page was my source.

 **Anonymous user's Opinion**

Rotary

 **Anonymous user's Opinion**

I don't recall

 **Anonymous user's Opinion**

St. Croix Sailing School

 **Anonymous user's Opinion**

This needs to be better communicated. I had no idea you were actually at this step and I missed a public meeting.....and I am a somewhat involved citizen. Now that we don't have the Star Observer- I'm not sure how you can get the message out there!

 **Anonymous user's Opinion**

Nextdoor Neighborhood

 **Anonymous user's Opinion**

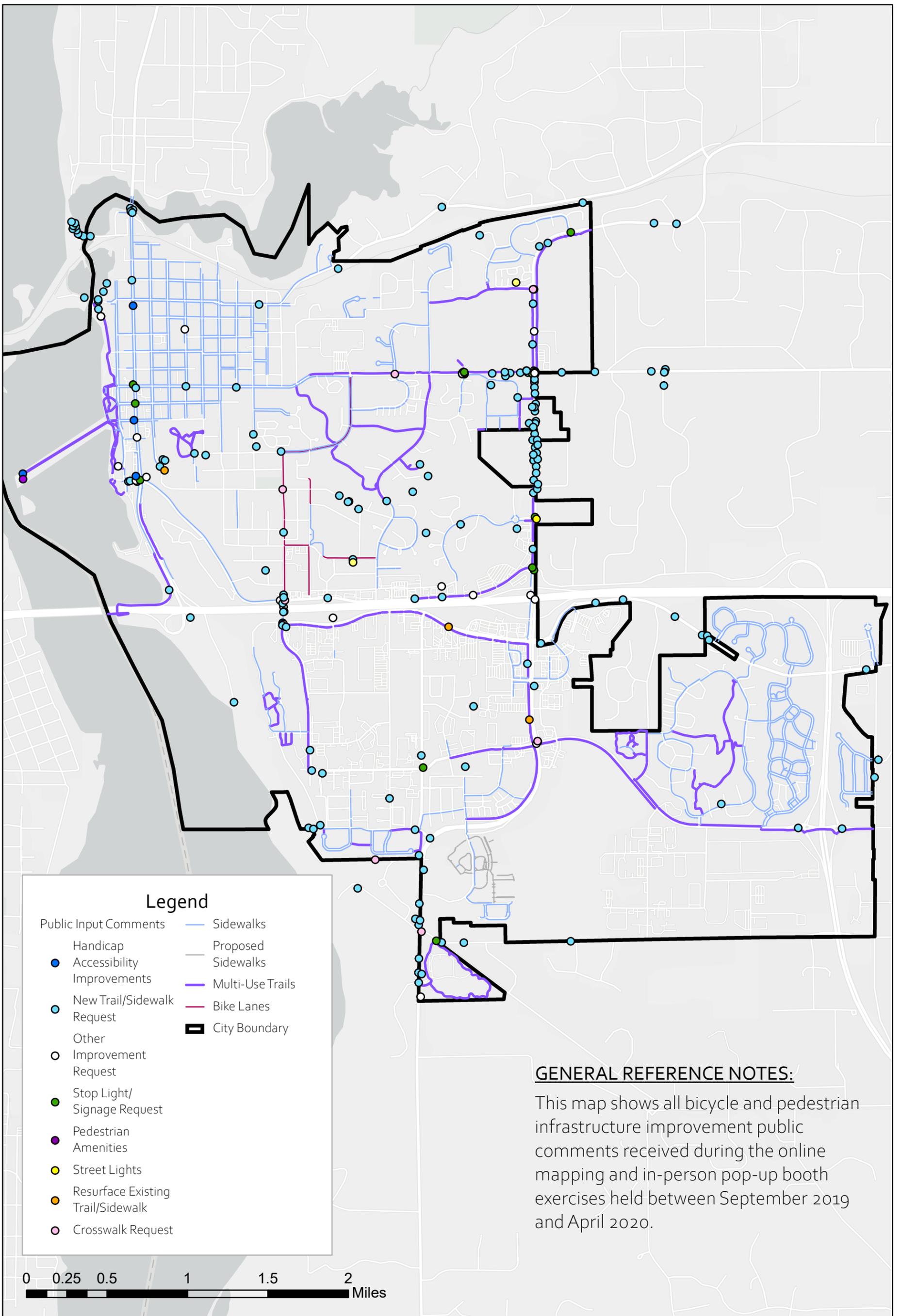
A volunteer provided me with a flyer at a table through the YMCA.

 **Anonymous user's Opinion**

saw the link in the Hudson Comprehensive Plan Steering Committee Meeting agenda

# APPENDIX A-3

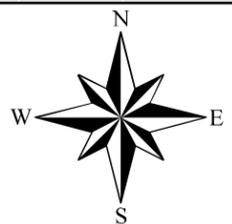
## Mapping Exercise



City Hall  
 505 3rd Street  
 Hudson WI 54016

Map (c) City of Hudson  
 Data (c) City of Hudson, & ESRI (base map)  
 Date Printed: 5/18/2021 3:04 PM

**PUBLIC FEEDBACK**  
 APRIL 2021  
 Hudson, Wisconsin



This data (i) is furnished "AS IS" with no representation as to completeness or accuracy; (ii) is furnished with no warranty of any kind; and (iii) is not suitable for legal engineering or surveying purposes. The City of Hudson shall not be liable for any damage, injury or loss resulting from this data.

# **APPENDIX B-1**

## **Crash Analysis**

# Hudson Bike & Ped Crashes 2010-2022

DOCTNMBR	ACCDATE	YEAR	NTRYHOUR	MUNITYPE	ONHWYRTP	ONHWYDR	ONHWY	ONSTR	ATHWY	ATSTR	ATNMBR	INTDIR	INTDIS	BIKEFLAG	PEDFLAG	TRVLDIR1	DRVRDO1	POSTSPD1	AGE1	SEX1	TRVLDIR2	DRVRDO2	SEX2	SAFETY2	
DZL30HP	03/08/2015	2015	19 C					CENTER DR	CARMICHAEL RD				0	Y	W	GO STR	25	21 M		GO STR	N	NA			
4GL04VVD6Z	05/05/2019	2019	18 C					E CANYON DR	LOUGHNEY BAY			N	3 Y		N	GO STR	25	38 F	E	BLNK	M				
4GL04BTWWK	03/10/2020	2020	18 C					BRIDGEWATER TRL	AMHERST CIR			E	10 Y		S	GO STR	25	32 F	W	BLNK	F				
DZNG3ZH	08/14/2011	2011	18 C					O KEEFE RD	WEBSTER ST				0 Y		N	GO STR	35	30 F	E	GO STR	M	NA			
DZNF3JZ	05/07/2015	2015	16 C					BURL OAK CURVE	CARMICHAEL RD	ROADWAY			0	Y	S	GO STR	25	17 M		OTHR	M	NA			
DZNF1V	07/08/2014	2014	10 C					17TH ST S	SUMMER ST			N	1 Y	Y	N	GO STR	25	72 F		GO STR	F	NA			
A316710	04/13/2010	2010	17 C					WARD AVE	13 ST S			E	1 Y		S	BACKNG	25	0 N	E	GO STR	M	NA			
DZLWFVZ	07/01/2013	2013	14 C	35 N				VINE ST	35 2ND ST			E	1 Y		W	RT TRN	25	74 F	S	GO STR	M	HLMT			
4GL045WRCC	08/11/2017	2017	13 C					CREST VIEW DR	INDUSTRIAL ST				0 Y		N	RT TRN	25	62 F	W	BLNK	M				
DZNLK2P	10/13/2015	2015	17 C	35 N			35	VINE ST	2ND ST				0	Y	S	LT TRN	25	47 M		OTHR	M	NA			
A316651	05/01/2010	2010	12 C					GRANDVIEW DR	SPRUCE DR			N	2	Y	N	GO STR	25	46 M		GO STR	M	NA			
A316689	07/06/2010	2010	18 C	35 N				NORTH ST	35 2ND ST				0 Y		E	STOPED	25	42 M	S	GO STR	F	NA			
A316625	05/24/2010	2010	14 C					CARMICHAEL RD	OAK RIDGE CIR				0	Y	N	GO STR	15	48 F		OTHR	M	NA			
DZNF1S	07/03/2014	2014	15 C					VINE ST	WISCONSIN ST				0 Y		W	RT TRN	25	42 F	S	GO STR	M	NA			
4GL03DG279	08/05/2017	2017	10 C					LAUREL AVE	11TH ST S				0 Y		W	GO STR	25	35 M	S	BLNK	M				
4GL04D2XWQ	08/19/2017	2017	1 C					HEGGEN ST	CREST VIEW DR				0 Y			GO STR	0	N	W	BLNK	M				
A316740	03/17/2010	2010	9 C					CREST VIEW DR	INDUSTRIAL ST				0	Y	N	RT TRN	40	51 M		GO STR	M	NA			
4GL04BTWX7	11/14/2020	2020	1 C	35 N				LOCUST ST	35			W	0	Y	E	BLNK	25	0 N		BLNK	F				
DZLZJN5	05/28/2016	2016	21 C					MONT CROIX DR	HEGGEN ST				0 Y		W	RT TRN	25	18 F	S	GO STR	M	NA			
A316659	05/06/2010	2010	19 C					HEGGEN ST	CRESTVIEW DR			S	1 Y		S	GO STR	25	17 F	W	GO STR	M	NONE			
A432293	05/07/2011	2011	2 C					GALWAY CT	BURL OAK CURVE			E	9	Y	S	GO STR	25	42 M		BLNK	M	NA			
A316632	05/29/2010	2010	20 C					WISCONSIN ST	13 ST				0 Y		W	GO STR	25	19 M	N	LT TRN	M	NA			
DZMKXS	10/24/2016	2016	13 C	35 N			35	SB	VINE ST				0	Y	W	LT TRN	25	0 N		GO STR	M	NA			
DZNF2Z	10/13/2014	2014	1 C					INDUSTRIAL ST	LIVINGSTONE RD			S	1 Y		S	GO STR	25	0 N	S	GO STR	M	NONE			
DZMK2P4	11/20/2016	2016	1 C	35 N			35	2ND ST	WALNUT ST	501			0	Y	N	GO STR	25	0 N		OTHR	M	NA			
DZLXXCW	03/31/2015	2015	19 C					GATEWAY BLVD	CREST VIEW DR				0	Y	N	RT TRN	25	54 M		CHG LN	F	NA			
DZNG56H	09/03/2016	2016	9 C	35 N			35	SB	ST CROIX ST				0 Y		S	GO STR	25	62 M	W	GO STR	F	HLMT			
4GL01L4FC5	05/04/2018	2018	15 C	35 N			35		WALNUT ST				0	Y	E	LT TRN	25	18 F		BLNK	F				
C6N6PN0	10/23/2010	2010	23 C	94 E			94		FRONT ST			W	6	Y	E	GO STR	65	18 F		OTHR	M	NA			
DZLXXC3	06/06/2013	2013	12 C					7TH ST	ST CROIX ST				0 Y		N	GO STR	25	23 F	E	GO STR	F	NA			
4DL05NC3H7	10/30/2020	2020	15 C					CARMICHAEL RD	BURL OAK CURVE			S	0	Y	W	RT TRN	15	74 F	N	GO STR	M	SH/LP			
4GL08CTJQR	12/28/2019	2019	7 C	35 N			35		LOCUST ST			N	1	Y	S	GO STR	25	29 F		BLNK	M				
DZNG416	08/21/2012	2012	16 C					17TH ST S	WARD AVE				1 Y		E	BACKNG	25	26 M	S	GO STR	F	NA			
4GL044NQ8L	08/03/2020	2020	9 C					MAYER RD	O'KEEFE RD			E	0 Y		E	LT TRN	25	62 M	N	BLNK	N				
DZNKSD	09/01/2016	2016	14 C					11TH ST	LAUREL AVE				0	Y	S	GO STR	25	72 M		GO STR	M	NA			
4GL01L4FCQ	06/15/2018	2018	14 C	35 N			35		WALNUT ST			S	0	Y	S	LT TRN	25	25 M		BLNK	F				
4GL04BTWWQ	06/15/2020	2020	16 C					O'KEEFE RD	CREST VIEW DR			S	1 Y		N	RT TRN	77	16 M	E	BLNK	M				
DZNG429	03/25/2013	2013	7 C					3RD ST	WALNUT ST			N	1	Y	N	LT TRN	25	0 N		GO STR	F	NA			
DZNG545	06/29/2015	2015	16 C					CREST VIEW DR	INDUSTRIAL ST				0 Y		N	RT TRN	25	49 M	W	OTHR	F	NONE			
DZNF3ZP	11/26/2013	2013	19 C	35 N			35	2ND ST	WALNUT ST			S	2	Y	N	GO STR	25	46 M		GO STR	F	NA			
DZNGV0G	07/04/2015	2015	11 C					11TH ST S	COLONIAL DR			S	7 Y		W	LT TRN	30	21 F	S	GO STR	M	NA			
DZM72SG	03/10/2016	2016	17 C	35 N			35		VINE ST				0	Y	S	LT TRN	25	48 M		OTHR	F	NA			
DZNG436	08/29/2013	2013	15 C					LOCUST ST	4TH ST			N	1 Y		S	GO STR	25	78 F	W	GO STR	M	HLMT			
4GL08CTJPG	08/13/2018	2018	19 C					CREST VIEW DR	INDUSTRIAL ST				0 Y		N	SL/ST	25	63 M	W	BLNK	M				
DZKN2NR	08/08/2016	2016	12 C					O'KEEFE RD	HANLEY RD				0	Y	E	GO STR	25	16 F		LT TRN	M	NA			
DZKNSZ3	11/28/2016	2016	17 C	35 N			35		VINE ST				0	Y	S	LT TRN	25	24 F		OTHR	F	NA			
DZM6BF7	10/11/2012	2012	9 C	35 N			35	2ND ST	COULEE RD	35 2ND ST S		E	1 Y		E	LT TRN	30	32 M	E	GO STR	M	NA			
DZLVPPV	09/09/2013	2013	15 C	35 N			35	2ND ST	WALNUT ST				0	Y	N	LT TRN	25	18 M		GO STR	F	NA			
4GL0474SGG	05/01/2017	2017	15 C	35 N			35		ST CROIX ST				0	Y	S	BLNK	15	17 F		BLNK	F				
4GL04BTWRC	01/03/2017	2017	15 C	35 N			35		VINE ST				0	Y	S	BLNK	25	17 F		BLNK	F				
DZN01MR	10/03/2011	2011	13 C	35 N			35	2ND ST	LOCUST ST				0	Y	N	GO STR	25	55 M		GO STR	M	NA			
DZLWVFZ	07/03/2012	2012	12 C					CENTER DR	CARMICHAEL RD				0 Y		E	RT TRN	25	79 F	N	GO STR	M	HLMT			
4GL03W8HKK	11/24/2020	2020	8 C				35		VINE ST			S	1	Y	S	LT TRN	25	32 F		BLNK	M				
4GL04HS26R	05/09/2017	2017	9 C					MAXWELL DR	PEARSON DR			S	2	Y	N	BLNK	25	54 M		BLNK	M				
DZNG3RK	02/14/2011	2011	15 C	35 N			35		WALNUT ST	35 2ND ST			0	Y	W	RT TRN	25	46 F		GO STR	F	NA			
	9/1/2021	2021						STAGELINE ROAD	CARMICHAEL RD				Y												
	4/15/2022	2022						Parking lot	O'KEEFE RD						Y										
	10/31/2021	2021						HERITAGE BLVD	STAGELINE RD						Y										

**KEY:**

**DOCTNMBR:** Documented Case Number  
**ACCDATE:** Date of Accident  
**YEAR:** Year Accident Occurred  
**NTRYHOUR:** The one hour range in which police were notified of crash  
**MUNITYPE:** Code describing municipality type (city, town, or village)  
**ONHWYRTP:** Three character route number for ONHWY  
**ONHWY:** Name of highway on which crash took place  
**ONSTR:** Name of local street on which crash took place  
**ATHWY:** Name of nearest highway where crash took place  
**ATSTR:** Name of intersecting street where crash took place

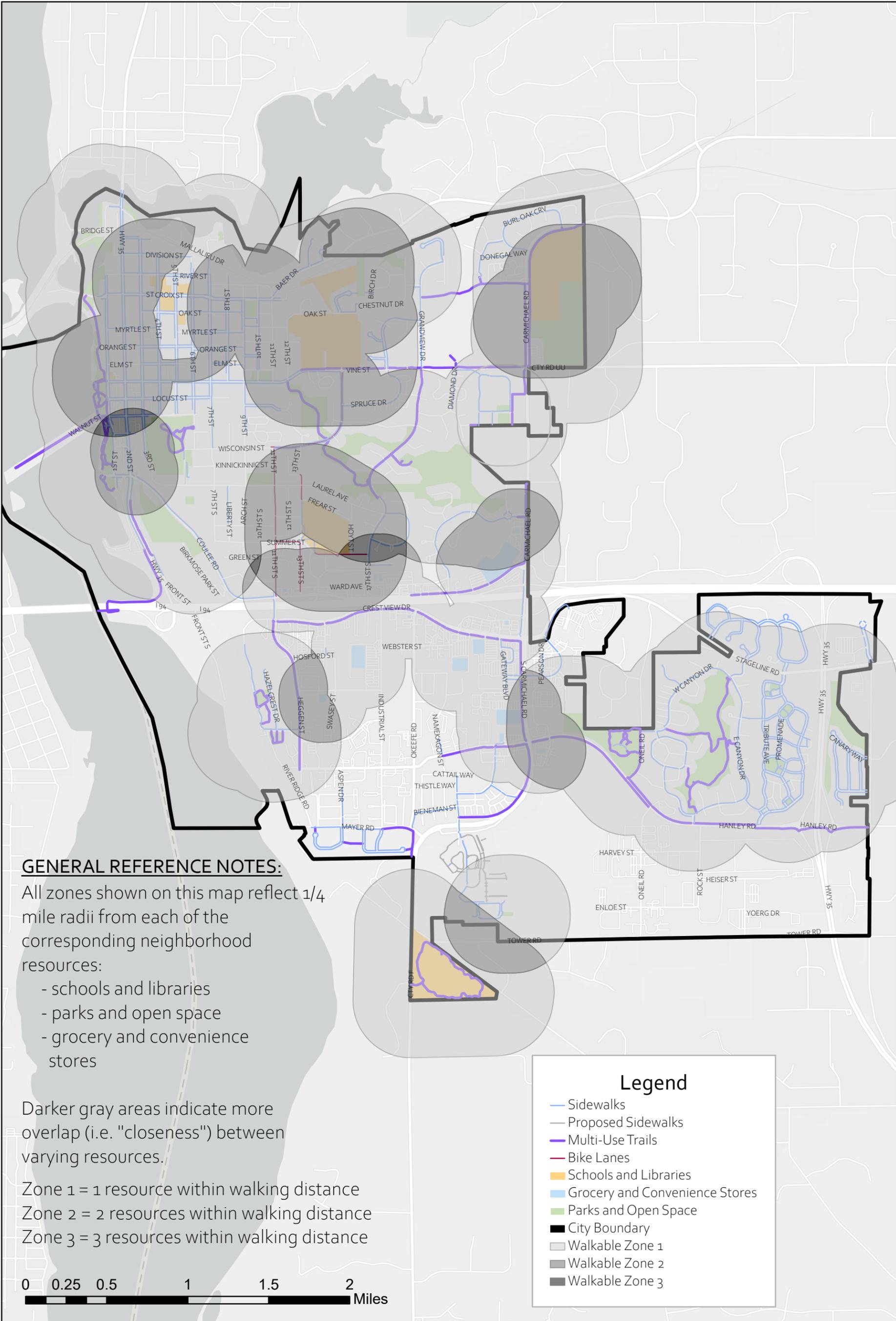
**ATNMBR:** House, fire, railroad or other number associated with crash location  
**INTDIR:** Cardinal direction of distance of intersecting roadway nearest crash site  
**INTDIS:** Intersection distance in hundredths of a mile from crash site  
**BIKEFLAG:** Bike involved accident  
**PEDFLAG:** Pedestrian involved accident  
**TRVLDIR1:** Travel direction of first individual involved in crash  
**DRVRDO1:** What first individual was doing at time of crash  
**POSTSPD1:** Posted speed limit for first individual involved in crash  
**AGE1:** Age of first individual involved in crash  
**SEX1:** Gender of first individual involved in crash

**TRVLDIR2:** Travel direction of second individual involved in crash  
**DRVRDO2:** What second individual was doing at time of crash  
**SEX2:** Gender of second individual involved in crash  
**SAFETY2:** safety precautions taken by second individual involved in crash

# APPENDIX C-1

Walkability Analysis

Map of Results



**GENERAL REFERENCE NOTES:**

All zones shown on this map reflect 1/4 mile radii from each of the corresponding neighborhood resources:

- schools and libraries
- parks and open space
- grocery and convenience stores

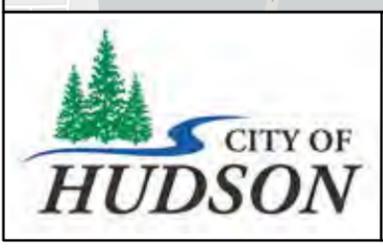
Darker gray areas indicate more overlap (i.e. "closeness") between varying resources.

- Zone 1 = 1 resource within walking distance
- Zone 2 = 2 resources within walking distance
- Zone 3 = 3 resources within walking distance



**Legend**

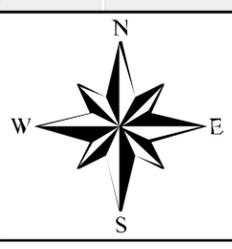
- Sidewalks
- Proposed Sidewalks
- Multi-Use Trails
- Bike Lanes
- Schools and Libraries
- Grocery and Convenience Stores
- Parks and Open Space
- City Boundary
- Walkable Zone 1
- Walkable Zone 2
- Walkable Zone 3



City Hall  
505 3rd Street  
Hudson WI 54016

Map (c) City of Hudson  
Data (c) City of Hudson, ESRI (base map)  
Date Printed: 1/15/2021 3:29 PM

**WALKABILITY ZONES**  
FEBRUARY 2021  
Hudson, Wisconsin



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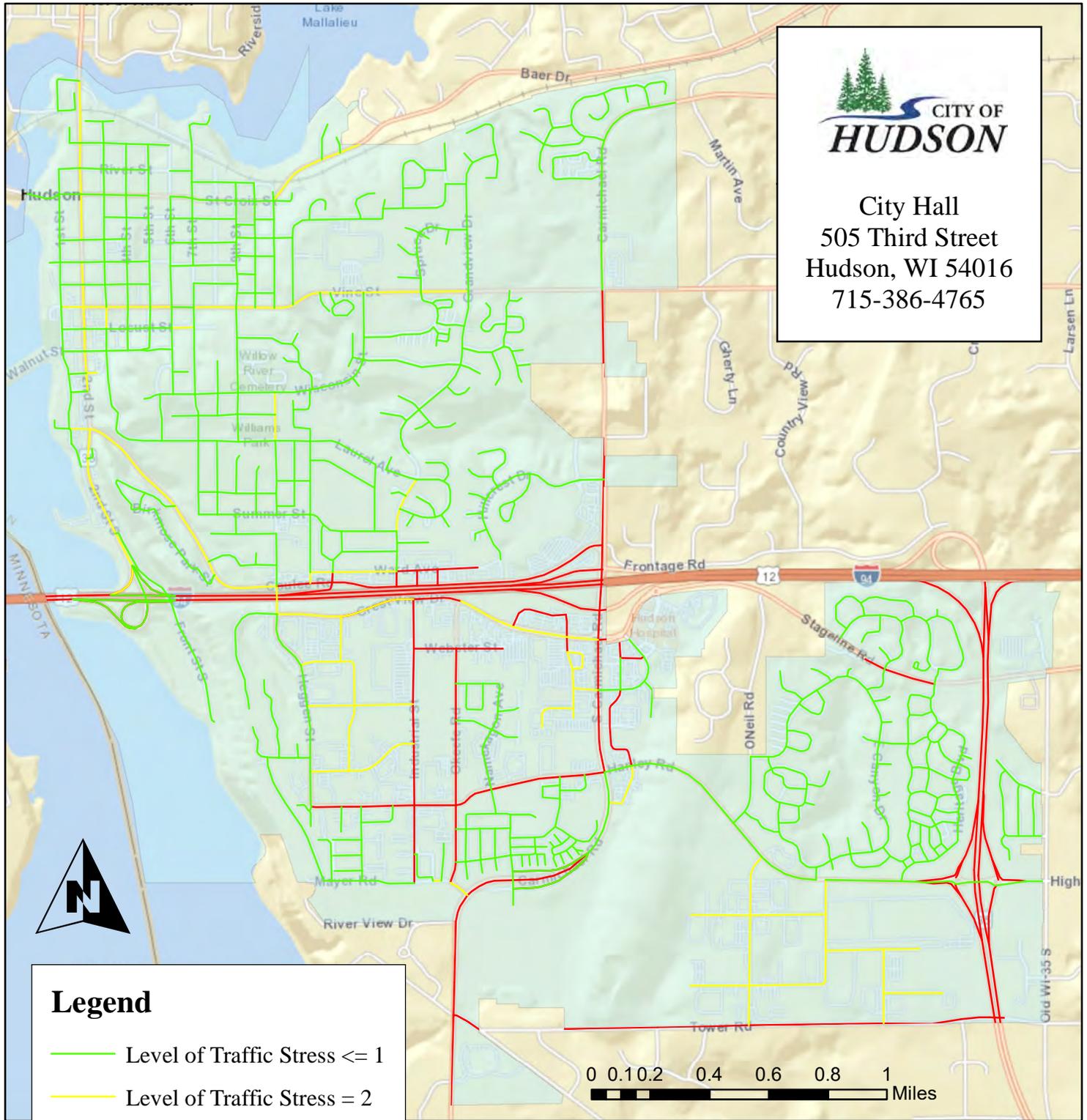
# **APPENDIX C-2**

## **Level of Traffic Stress (LOTS) Analysis Results**

# Level of Traffic Stress Analysis for Bicyclists

## City of Hudson, Wisconsin

### City-Wide View



  
**CITY OF HUDSON**  
 City Hall  
 505 Third Street  
 Hudson, WI 54016  
 715-386-4765

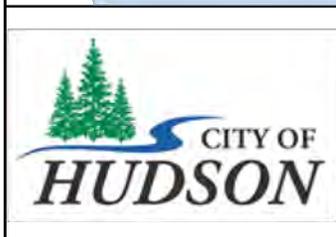
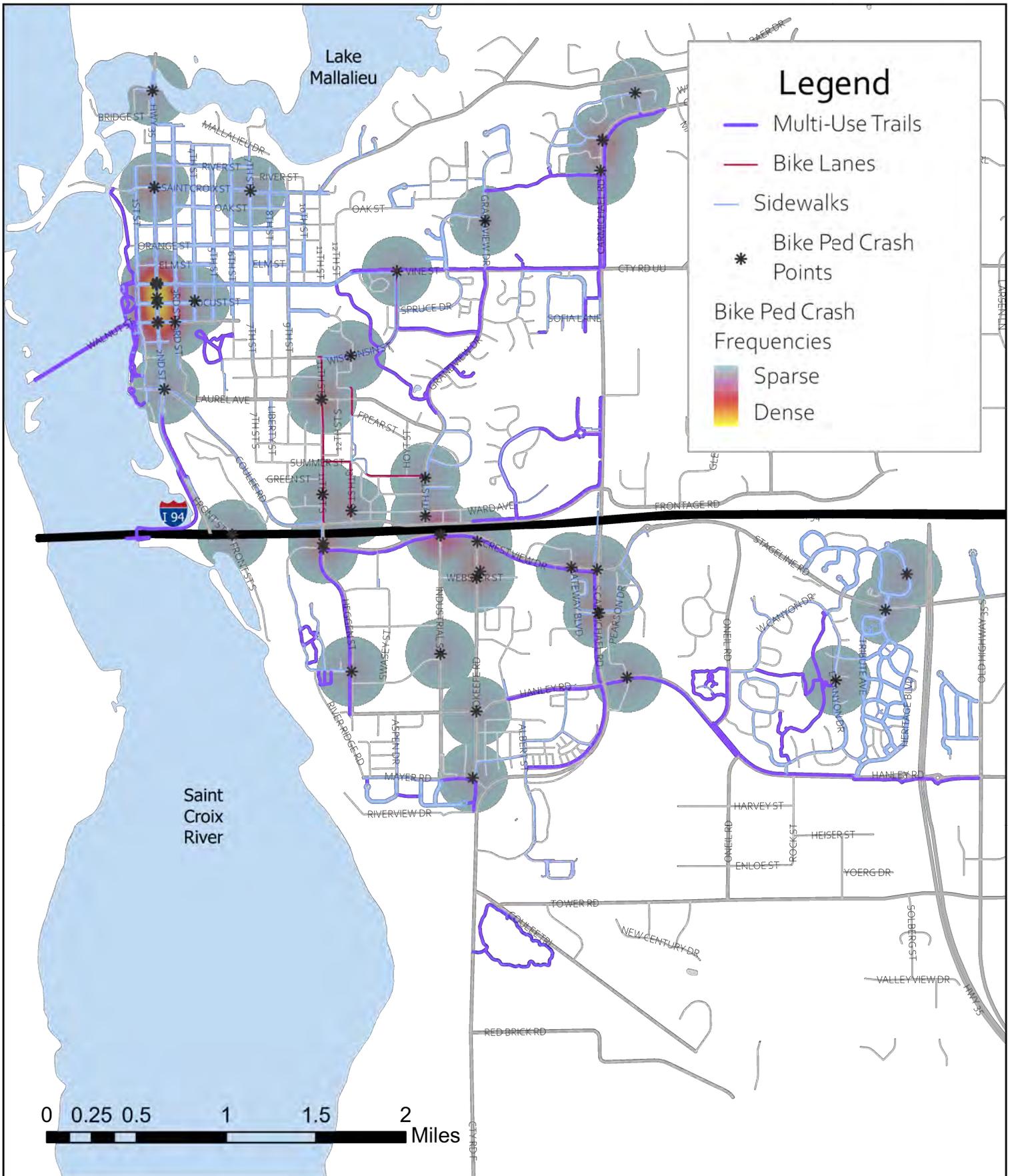
**Legend**

- Level of Traffic Stress  $\leq 1$
- Level of Traffic Stress = 2
- Level of Traffic Stress  $\geq 3$
- City Boundary

Map: © City of Hudson (2021) Data: City of Hudson (2021); SCCWI (2020)

# APPENDIX C-3

## Crash Analysis Heat Map



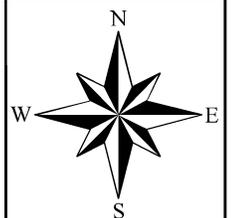
City Hall  
505 3rd Street  
Hudson WI 54016

Map (c) City of Hudson  
Data (c) City of Hudson, & ESRI (base map)

Date Printed: 5/5/2022 11:14 AM

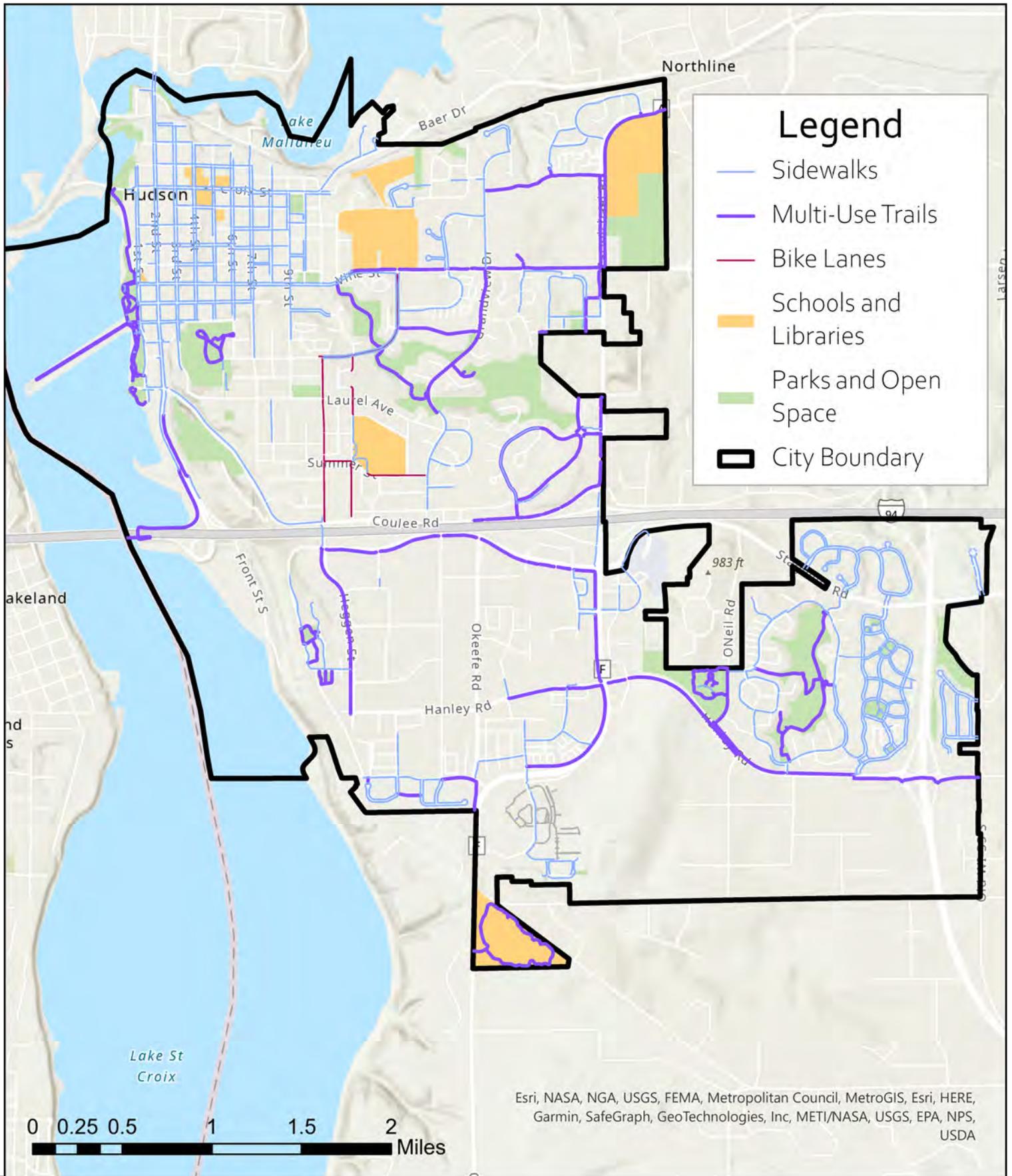
## Bike Ped Incidences (2010-2022)

May 2022  
Hudson, Wisconsin



# APPENDIX C-4

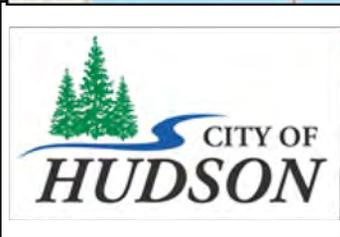
## Existing Bicycle and Pedestrian Infrastructure Map



### Legend

- Sidewalks
- Multi-Use Trails
- Bike Lanes
- Schools and Libraries
- Parks and Open Space
- City Boundary

Esri, NASA, NGA, USGS, FEMA, Metropolitan Council, MetroGIS, Esri, HERE, Garmin, SafeGraph, GeoTechnologies, Inc, METI/NASA, USGS, EPA, NPS, USDA



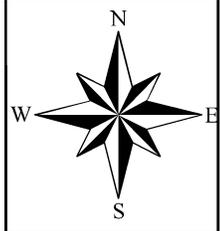
City Hall  
505 3rd Street  
Hudson WI 54016

Map (c) City of Hudson  
Data (c) City of Hudson, & ESRI (base map)

Date Printed: 6/3/2022 3:56 PM

# Trails and Sidewalks

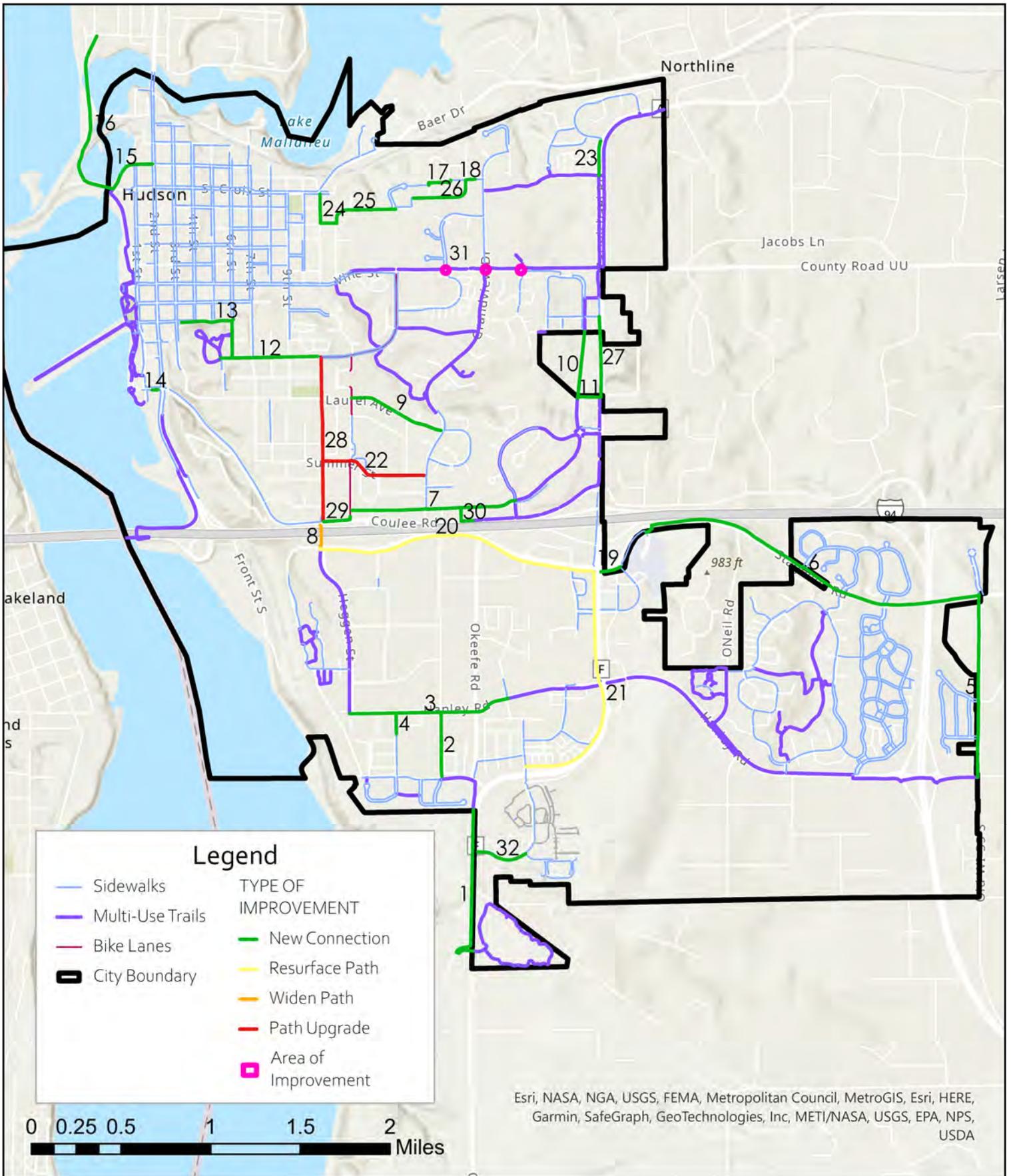
June 2022  
Hudson, Wisconsin



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# APPENDIX C-5

## Proposed Infrastructure Improvements Map

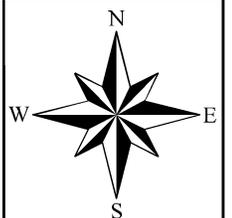


City Hall  
505 3rd Street  
Hudson WI 54016

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# Proposed Bike Ped Improvements

June 2022  
Hudson, Wisconsin



# APPENDIX D-1

## Proposed Signage Drafts

MULTI-USE TRAIL

# VINE STREET



MULTI-USE TRAIL

# LAKERFRONT PARK



MULTI-USE TRAIL

# COULEE ROAD



MULTI-USE TRAIL

# COULEE ROAD



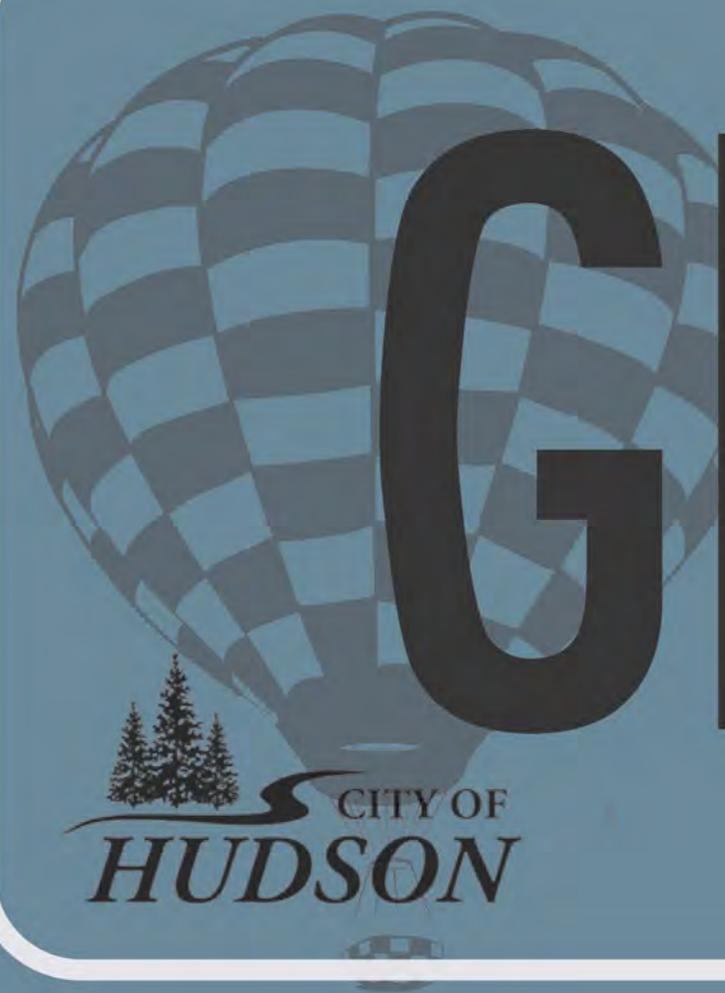
MULTI-USE TRAIL

# GRANDVIEW DRIVE



MULTI-USE TRAIL

# GRANDVIEW DRIVE



MULTI-USE TRAIL

# CARMICHAEL ROAD



MULTI-USE TRAIL

# CARMICHAEL ROAD



MULTI-USE TRAIL

# HANLEY ROAD



MULTI-USE TRAIL

# HEGGEN STREET



MULTI-USE TRAIL

# CRESTVIEW DRIVE



MULTI-USE TRAIL

# CRESTVIEW DRIVE



MULTI-USE TRAIL

# COON'S HILL PARK



MULTI-USE TRAIL

# COON'S HILL PARK



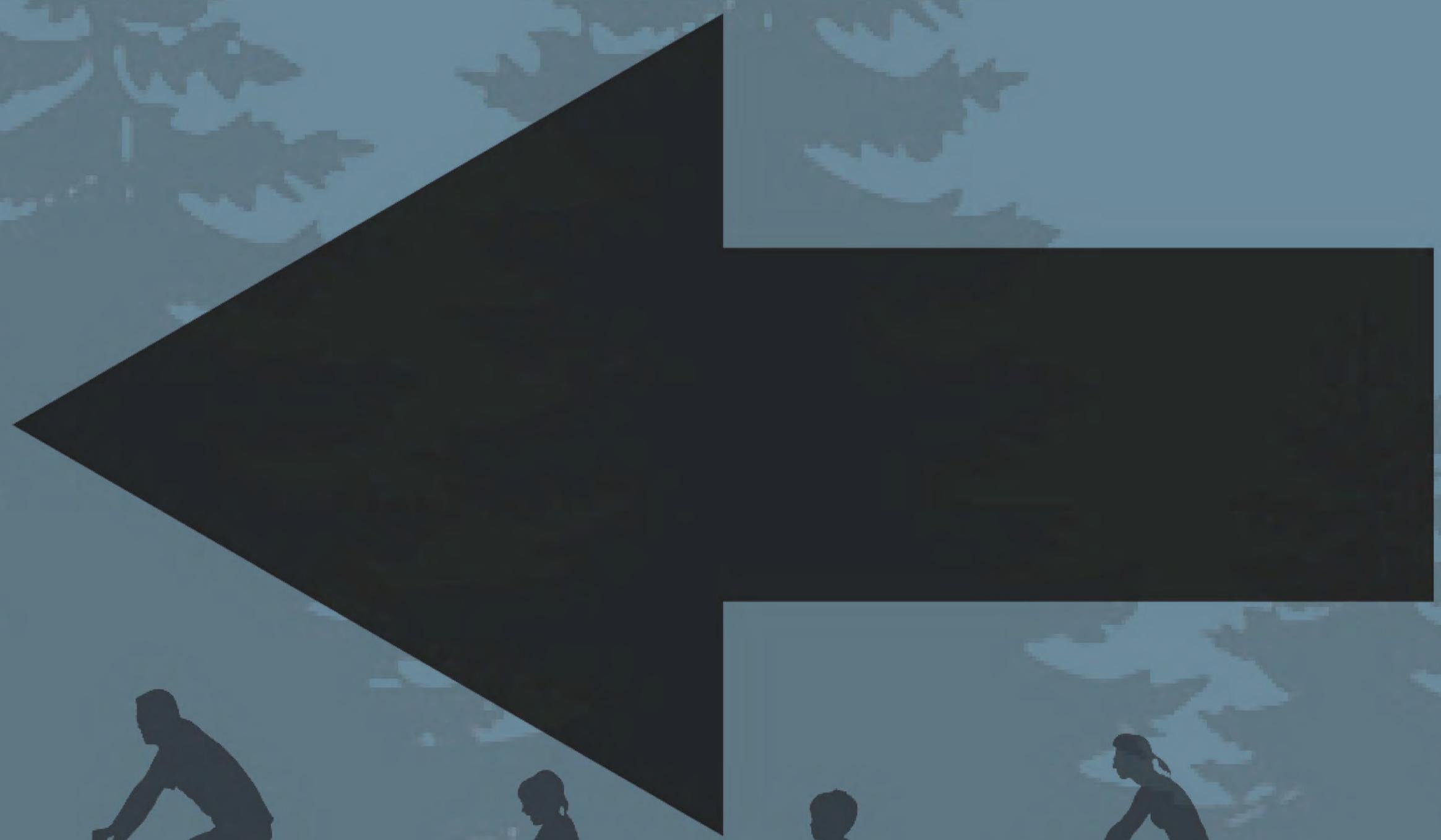
MULTI-USE TRAIL

# HUNTER HILL

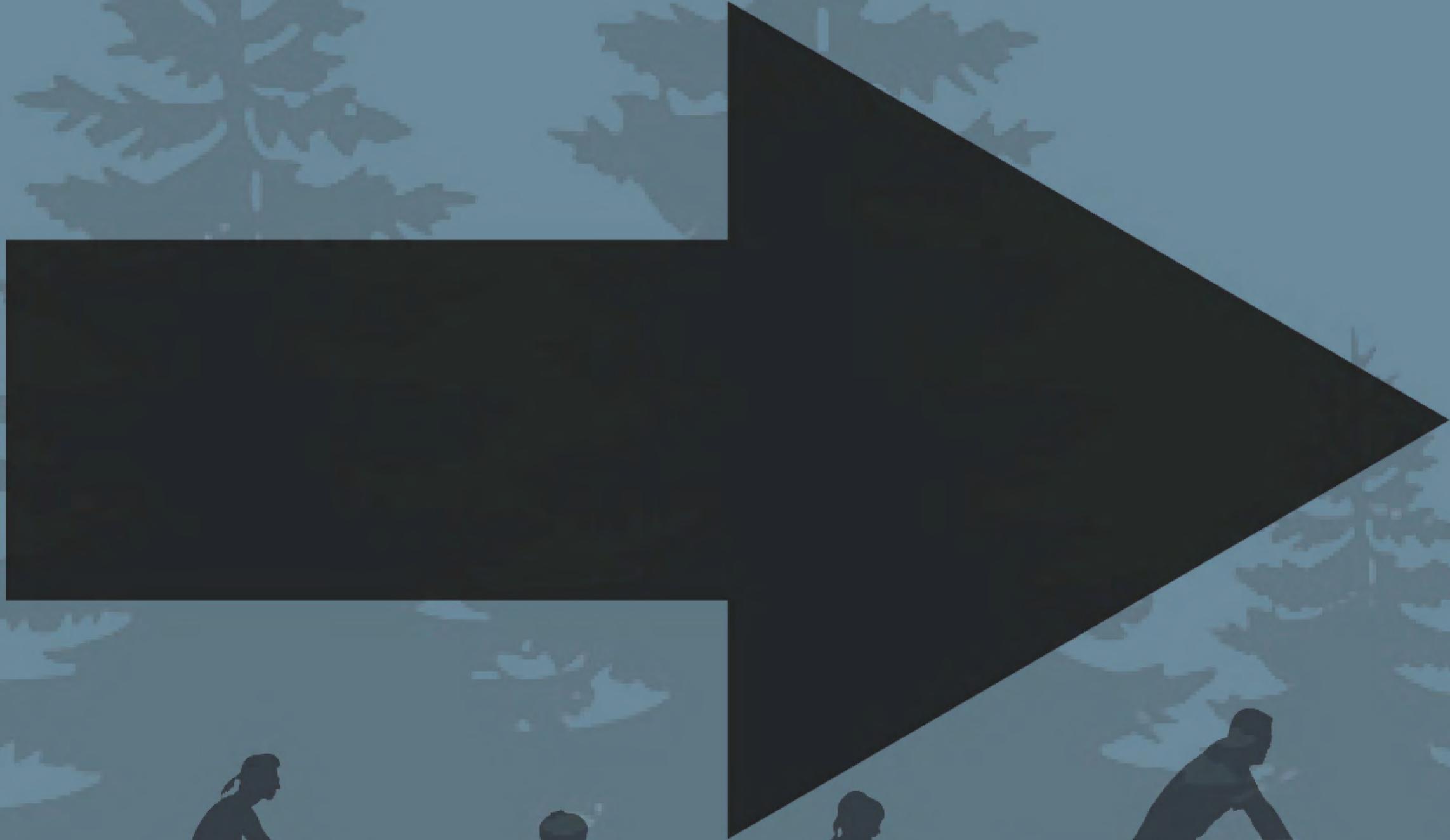














# **APPENDIX E-1**

## Complete Streets Model Policy

# Complete Streets Model Policy Language

(as drafted by St Croix County, WI)

## Section 1: Vision

This Complete Streets policy directs the [MUNICIPALITY] to provide streets that are safe and accessible for all people. Complete Streets will benefit the community in many ways, including enhancing quality of life and creating a balanced and interconnected transportation network that provides for economically sound and connected development patterns, public health and safety, livability, equity, affordability, economic activity, and community character.

### Modifications to Section 1

The vision should be customized or tailored for each individual community based on their unique values and goals. Communities are encouraged to create a completely new vision for their policies based on the Complete Streets principles included throughout this model policy.

## Section 2: All Users and Modes

It is the intent of the [MUNICIPALITY] that Complete Streets and roads be safe for users of all ages, all abilities and all income levels as a matter of routine. This Policy directs decision-makers to consistently plan, design, construct, and maintain streets to accommodate all anticipated users including but not limited to pedestrians, bicyclists, motorists, emergency vehicles, and [INSERT OTHER USERS AS APPROPRIATE].

### Modifications to Section 2

This section should be modified to include all anticipated modes in a community that should be considered during street design (although every mode may not necessarily be accommodated on every street). Example additions include paratransit, freight and commercial vehicles, and agricultural vehicles.

## Section 3: All Projects and Phases

All types of transportation projects are subject to this policy, regardless of funding source, including those involving new construction, reconstruction, retrofit, repaving, rehabilitation, and change in the allocation of pavement space on an existing street.

### Modifications to Section 3

Some communities may feel that this language is too far-reaching. However, it is recommended that Section 3 not be modified. Rather, modify the exceptions that are included in Section 4 in order to improve the efficiency of this policy.

## Section 4: Clear, Accountable Exceptions

Any exception to this policy must be approved by the [TOWN/VILLAGE BOARD, CITY COUNCIL, TRANSPORTATION COMMITTEE, or PUBLIC WORKS COMMITTEE] and be documented with supporting data that indicates the basis for the decision. Such documentation shall be publicly available.

Exceptions may be considered for approval when:

1. An affected roadway prohibits, by law, use by specific users (such as interstate highways) in which case a greater effort shall be made to accommodate those specified users elsewhere, including on roadways that cross or otherwise intersect with the affected roadway;
2. The costs of providing accommodations are excessively disproportionate to the need or probable use;
3. The existing and planned population, employment densities, traffic volumes, or level of transit service around a particular roadway is so low as to demonstrate an absence of current and future need.
4. Transit accommodations are not required where there is no existing or planned service;
5. Routine maintenance of the transportation network does not change the roadway geometry or operations, such as mowing, sweeping, and spot repair;
6. There is a reasonable and equivalent project along the same corridor that is already programmed to provide facilities exempted from the project at hand.

### Modifications to Section 4

Each municipality should identify the appropriate body for overseeing the application of this policy and approving or denying exceptions. The list of exceptions may be modified, but should be done so with careful consideration so as not to render the policy ineffectual. The rule of thumb definition for “excessively disproportionate” (Exception 2) is 20 percent of the total project cost. This number is not hard and fast as the appropriate figure may be substantially higher or lower on any given project, but communities should not consistently define excessively disproportionate as a figure substantially lower than 20 percent.

## Section 5: Network

The [MUNICIPALITY] recognizes the need for a connected, integrated network for all modes that improves street connectivity and provides transportation options to a resident’s many potential destinations. This policy recognizes that all modes do not receive the same type of accommodation or amount of space on every street, but that the street network should allow everyone to safely and conveniently travel across the community.

### Modifications to Section 5

It is important that policies recognize that Complete Streets are not stand-alone projects, but are part of a greater interconnected system. The language in this section can be modified or combined with language in another section.

## Section 6: Jurisdiction

This policy applies the [MUNICIPALITY], private developers, St. Croix County, the Wisconsin Department of Transportation, and any other body that constructs or maintains streets and roads within the incorporated boundaries of the [MUNICIPALITY].

### Modifications to Section 6

Many agencies and organizations play a role in the development of a community's transportation network. It is important that a municipality's policy recognize this fact and express its applicability to projects performed by other agencies.

## Section 7: Design

The [MUNICIPALITY] and any agency or organization that plans, designs, or constructs a transportation facility in the incorporated boundaries will use the latest and best design standards when designing streets, including the latest editions of the following:

### Modifications to Section 7

The list of standards and guidelines can be modified. This list should not be seen as a mandate to consult every publication on every project.

- **General Street Design**

- o Designing Walkable Urban Thoroughfares: A Context Sensitive Approach: An ITE Recommended Practice (Institute of Transportation Engineers)
- o Urban Street Design Guide (National Association of City Transportation Officials)

- **Bicycle and Pedestrian Design**

- o Guide for the Development of Bicycle Facilities (American Association of State Highway and Transportation Officials)
- o Wisconsin Bicycle Facility Design Handbook (Wisconsin Department of Transportation)
- o St. Croix County Bicycle and Pedestrian Plan Design Guidelines (St. Croix County)
- o Urban Bikeway Design Guide (National Association of City Transportation Officials)
- o Guide for the Planning, Design, and Operation of Pedestrian Facilities (American Association of State Highway and Transportation Officials)
- o Public Rights-of-Way Accessibility Guidelines (United States Access Board)

- **Detailed Street Design**

- o A Policy on Geometric Design of Highways and Streets (American Association of State Highway and Transportation Officials)
- o Manual on Uniform Traffic Control Devices (Federal Highway Administration)

## Section 8: Context Sensitivity

Complete Streets will be designed in a context-sensitive manner to respond to the character of the surrounding neighborhood, its current and planned buildings, as well as its current and expected transportation needs.

### Modifications to Section 8

It is important that policies recognize that Complete Street design must be sensitive to context. The language in this section can be modified or combined with language in another section.

## Section 9: Performance Measures

The [MUNICIPALITY] will measure the success of this policy using various performance measures, including but not limited to:

- Number of crashes and severity of injuries
- Injuries and fatalities for all modes
- Number of curb ramps
- Number of pedestrian countdown signals
- Miles of routes accessible for people with disabilities
- Sidewalk condition ratings
- Travel time in key corridors (point A to point B)
- Emergency vehicle response times
- Number of students who walk or bike to school
- Commercial vacancies in downtown
- Bike route connections to off-road trails (equity across all districts of the community)
- Citizen and business surveys of satisfaction with streets and sidewalks
- Number of bicycle friendly businesses recognized by the League of American Bicyclists
- Number of bike parking spaces

### Modifications to Section 9

This section should be modified to include a reasonable set of performance measures (potentially as few as two) that will help the community track progress. Consideration should be given to data availability and ease of tracking when selecting performance measures.

The [MUNICIPAL DEPARTMENT, ADMINISTRATOR, ETC] will present an annual report to the [TOWN/VILLAGE BOARD, CITY COUNCIL, TRANSPORTATION COMMITTEE, or PUBLIC WORKS COMMITTEE] showing progress made in implementing this policy.

## Section 10: Implementation Steps

Implementation of this policy will be carried out cooperatively among all departments in the [MUNICIPALITY] with multijurisdictional cooperation, and to the greatest extent possible, among private developers and state, regional, and federal agencies. The [MUNICIPALITY] will take specific steps to implement this policy, including:

1. Restructuring or revising related procedures, plans, regulations, and other processes to accommodate all users on every project, including:
  - a. [RELATED PROCEDURES, PLANS, REGULATIONS, and OTHER PROCESSES]
  - b. ...
  - c. ...
2. Adopting and regularly consulting the St. Croix County Bicycle and Pedestrian Plan Design

### Modifications to Section 10

This section should be modified based on the community's capabilities and priorities. The National Complete Streets Coalition encourages communities to include variations of the four specific steps included in this model policy language. In addition to adopting the St. Croix County Bicycle and Pedestrian Plan Design Guidelines, communities may also elect to adopt national or state-level recognized design guidance.

Guidelines, which reflect the current state of best practices in bicycle and pedestrian design.

3. Offering opportunities for transportation staff, community leaders, and the general public to participate in workshops and other training opportunities so that everyone understands the importance of the Complete Streets vision.
4. Developing and instituting better ways to measure performance and collect data on how well the streets are serving all users.